



# NEW BRITAIN BOROUGH MAIN STREET STUDY

DECEMBER 2016





# NEW BRITAIN BOROUGH

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Main Street Study

December 2016

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# NEW BRITAIN BOROUGH

## Main Street Study

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December 2016

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Appendix I Historic Properties

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# INTRODUCTION

## STUDY PURPOSE

The purpose of the Butler Avenue Main Street Study is to define a comprehensive vision that can promote the economic development and revitalization of the Butler Avenue corridor within New Britain Borough, create a more walkable and attractive “Main Street,” and more closely tie the community to the two accessible rail stations and to Delaware Valley University. The completion of the Route 202 Parkway in 2012, the proposed expansion of programs at Delaware Valley University, the closing of long-standing businesses, and the needs of other businesses have motivated Borough leaders to address the future of the Borough’s business climate through this study.

In 2015, the Borough secured a Transportation and Community Development Initiative grant from the Delaware Valley Regional Planning Commission to create a redevelopment/revitalization plan for the Butler Avenue corridor. This project was initiated after a nearly 2-year period of community visioning and outreach by Borough Council, the Borough Planning Commission, and the Community and Business Committee. Together with other community partners including local business owners, residents, and officials from Delaware Valley University, the beginnings of a common vision for the corridor had been formed.

Through discussion at community meetings, several items emerged as priority issues that are central to realizing the participants’ vision for the future. These themes serve as the foundation for this study:

- Encourage the creation of a pedestrian-friendly corridor along Butler Avenue and improve walkability and pedestrian access throughout the Borough;
- Encourage Borough residents and business owners to use Borough businesses for goods and services;
- Identify a site to establish a town center with public space and amenities for civic gatherings;
- Establish gateway features along Butler Avenue at the entrances of the Borough which could include signage and plantings;
- Continue Borough support of the significant existing commercial and industrial uses within the Borough; and
- Encourage the ongoing efforts to capitalize on the synergy between the Borough, its residents and business community, and Delaware Valley University.

## STUDY AREA

The study area has been primarily defined as those properties with street frontage along Butler Avenue as shown in Map 1. Other properties that are in close proximity to the corridor have been included in the study area owing to their potential for redevelopment and contribution to corridor development.

The study area was then divided into three segments for analysis, and subsequent recommendations that will be found in later sections of the study. These areas, from west to east, include the Town Center, the Historic Village, and the University Village areas.

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### *TOWN CENTER*

The Town Center area comprises the corridor from the Borough's boundary with Chalfont at Bristol Road and continues to Lenape Drive. Within the Town Center area, there are a mix of commercial and residential land uses from Bristol Road to Ute Road and between Ute Road and Lenape Drive on the north side of the corridor is the Town Center of New Britain shopping center.

Within the Town Center area, on the north side of Butler Avenue, are some of the larger single uses within the corridor including the Carousel Pointe and Forest Park residential developments, the Pine Run Elementary School, and the Lenape Valley Presbyterian Church. On the south side are a mixture of smaller scale commercial uses and some office uses.

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### *HISTORIC VILLAGE*

The Historic Village area is the section of the corridor from Lenape Drive to just west of Beulah Road. This area encompasses the heart of the Borough's historic village as well as established residential areas. There are some nonresidential land uses within this area but it is largely residential. Butler Avenue curves slightly as it travels through this part of the Borough. This, combined with the larger lot residential uses, gives this part of the Borough a more open and airy character as compared to the other two more densely developed segments.

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### *UNIVERSITY VILLAGE*

The University Village area is the eastern most part of the corridor and is from Beulah Road to the Borough boundary near New Britain Road. This area includes residential and nonresidential uses, with a mix of commercial and office uses throughout the corridor. The campus of Delaware Valley University dominates the landscape of this area.

# Butler Avenue Study Area

## Map 1



# EXISTING CONDITIONS

## REGIONAL SETTING

New Britain Borough began as a large village in New Britain Township. Incorporated in 1928, New Britain Borough stretches along Butler Avenue (old Route 202) for about 2¾ miles between Chalfont Borough and Doylestown Township. Its northern border extends to Covered Bridge Park on Keeley Avenue; its southern border roughly extends along Cooks Run to the west and the Lansdale/Doylestown commuter line to the east.

Butler Avenue is the major transportation route through the Borough. Running east-west, Butler Avenue connects New Britain to areas east in Bucks County and areas west in Montgomery County. Bristol Road enters the Borough from the south at its western border, terminating at Butler Avenue. In the center of the Borough, Tamenend Avenue cuts across Butler Avenue. Tamenend becomes Almshouse Road as it leaves the Borough to the south, briefly entering New Britain Township before moving into Doylestown Township. Adjacent to Tamenend on the north side of Butler Avenue, Keeley Avenue connects New Britain Borough to Ferry Road in New Britain Township.

The SEPTA Doylestown commuter rail line runs through the south part of the Borough and connects Bucks and Montgomery county communities with the City of Philadelphia. Stations located along the Doylestown route in Bucks County include Doylestown (the terminus of the line), Delaware Valley University, New Britain, and Chalfont. New Britain station is located just south of the Borough's border on Tamenend Avenue.

## POPULATION AND DEMOGRAPHIC TRENDS

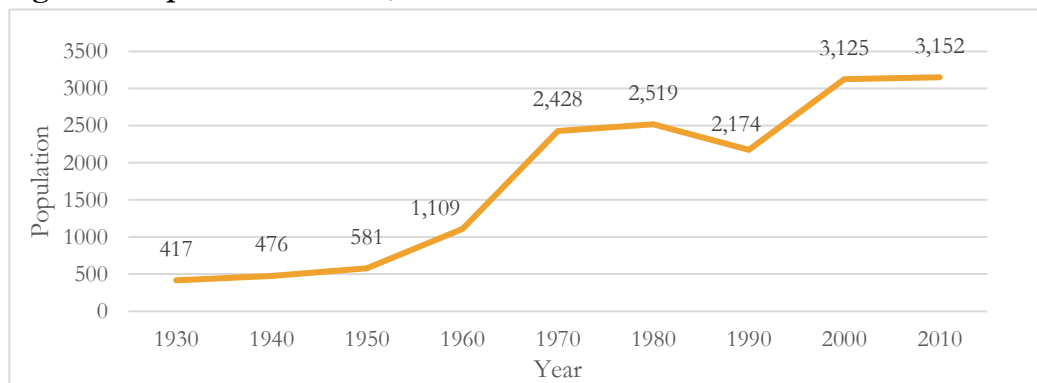
Basic demographic measures of population and housing conditions, both past and present, can provide some sense of the key characteristics of a community and an indication of where it is headed. The Borough is a nearly built-out community with a stable population and demographic base since 2000. Rapid growth that occurred in surrounding municipalities and within Central Bucks County did not have a great impact on the Borough. Even though there has been little change other than Delaware Valley University student population, it is still important to assess current conditions and potential trends for the future in order to plan for possible growth and change.

In order to understand the trends that affect the Borough, an examination of data from several geographic levels will be presented. These geographies will include the municipal boundaries of the Borough and the neighboring municipalities as well as a drive-time analysis of the market area. The drive-time areas are the surrounding geography of the market divided into 0-5, 0-10, and 0-15-minute drive-times<sup>1</sup> from the center of the Borough (which for the purposes of this study was determined to be at the intersection of Butler Avenue and Tamenend Avenue).

### POPULATION

A basic and primary indicator used to determine where a community is headed is often population growth over time. According to 2010 U.S. Census data, there were 3,152 people residing within the Borough's 1.22-square miles. The Borough has experienced population growth nearly every decade over the past 80 years, with only the decade from 1980 to 1990 showing a loss. Figure 1 shows the Borough's historic population growth since 1930. The Borough experienced its greatest growth during the two 10-year periods from 1950 to 1960 and 1960 to 1970 where it grew 90.9 percent and 118.9 percent respectively.

**Figure 1. Population Growth, 1930 to 2010**



Source: U.S. Census Bureau

<sup>1</sup> Drive-time intervals are areas defined by distance that can be driven away from a specific location within a specified time (in minutes) assuming posted speed limits for the road network. Barriers such as mountains, rivers, bridges, or highways under normal traffic conditions are taken into account when establishing the boundaries.

**Table 1. Population by Area/Region/County/State**

	1990	2000	2010	Change 1990 to 2010	Percentage Change
New Britain Borough	2,174	3,125	3,152	978	44.9
<b>Adjacent Municipalities</b>					
New Britain Township	9,099	10,698	11,070	1,971	21.6
Chalfont Borough	3,069	3,900	4,009	1,030	33.5
Doylestown Township	14,510	17,619	17,565	3,055	21.0
<b>Total</b>	<b>26,678</b>	<b>32,217</b>	<b>32,644</b>	<b>6,056</b>	<b>22.7</b>
<b>Central Bucks Region<sup>2</sup></b>	<b>187,991</b>	<b>228,532</b>	<b>247,095</b>	<b>59,110</b>	<b>31.4</b>
<b>Bucks County</b>	<b>541,224</b>	<b>597,635</b>	<b>625,249</b>	<b>84,025</b>	<b>15.5</b>
<b>DVRPC Nine-County Region<sup>3</sup></b>	<b>5,182,705</b>	<b>5,386,867</b>	<b>5,626,186</b>	<b>443,481</b>	<b>8.5</b>

*Source: U.S. Census Bureau, Delaware Valley Regional Planning Commission*

The population within New Britain Borough increased by nearly 45 percent between 1990 and 2010 as shown in Table 1 above. This large population increase is due to the counting of groups of residents in institutional settings such as Delaware Valley University.

The Census Bureau has released 2015 population estimates that show the Borough's population as 3,017. This is a decrease of 135 people from the 2010 Census. This decline in population is most likely attributed to the continued decrease in average household size, an aging population, very little development proposals, and the fluctuation in the number of university students.

It is also useful to compare the Borough's population changes to those experienced in the region in order to gain insight into regional trends affecting the Borough. Table 1 shows the population for the four adjacent communities, the Central Bucks County Region, Bucks County, and the nine-county Delaware Valley Regional Planning Commission (DVRPC) region.

## RACE AND ETHNICITY

Other characteristics pertinent to understanding the nature of a community involve its racial and ethnic composition. New Britain Borough is relatively homogeneous in terms of race and ethnicity with 94.3 percent of residents identifying themselves as white. In comparison, county residents are 89.2 percent white. Over the past 15 years the number of Borough residents who consider themselves nonwhite has increased in both number and percentage of total population (Table 2).

<sup>2</sup> Central Bucks Region includes Buckingham, Doylestown, New Britain, Newtown, Northampton, Plumstead, Solebury, Upper Makefield, Upper Southampton, Warminster, Warrington, Warwick, and Wrightstown townships and Chalfont, Doylestown, Ivyland, New Britain, New Hope, and Newtown boroughs.

<sup>3</sup> The Delaware Valley Regional Planning Commission (DVRPC) nine-county region includes the counties of Bucks, Chester, Delaware, Montgomery and Philadelphia in Pennsylvania; and Burlington, Camden, Gloucester and Mercer in New Jersey.

**Table 2. Nonwhite and Hispanic Population<sup>4</sup>**

New Britain Borough	2000				2015			
	Non- white	Percent of Total	Hispanic or Latino	Percent of Total	Non- white	Percent of Total	Hispanic or Latino	Percent of Total
	129	4.1	24	1.6	207	6.7	101	3.3

*Source: U.S. Census Bureau, ESRI Business Analyst*

## HOUSEHOLDS AND FAMILIES

The number of households in New Britain totaled 908 in 2015, and 69.0 percent (627) of these households were family households. In 2010, the number of households was 912, with 73.4 percent of these being family households. Household<sup>5</sup> and family size has been declining nationwide in recent years due to a number of factors: later family formation, declining birth rates, rising divorce rates, and more young people and older people living alone. The average household size in the Borough in 2010 was 2.46 persons, down from 2.58 in 2000 and 2.76 in 1990. For comparison, in 2010, Bucks County's average household size was 2.63 persons.

Table 3 provides a summary of the characteristics of households in the Borough. Between 2000 and 2010, there was a 16 percent increase in family households without children, which may have an influence on housing and retail marketing strategies (further discussed in the Economic Development chapter).

**Table 3. Household and Family Characteristics**

Characteristic	2000	2010	2015
Number of Households	912	934	908
Average Household Size	2.58	2.46	2.43
Average Family Size	3.01	2.93	2.9
Family Households	670	651	627
Married Couple Families	555	527	NA
Non-Family Households	242	283	281
Householders Living Alone	204	231	NA

*Source: U.S. Census Bureau, ESRI Business Analyst*

<sup>4</sup> The 2000 Census featured a change which allowed individuals to select more than one race and which separated racial identification from Hispanic or Latino classification. The total in the table includes those who only selected one race and categorized themselves as "white." It does not include those who selected more than one race of which "white" was one of the categories selected.

<sup>5</sup> A household is one or more persons living in a single housekeeping (dwelling) unit. A family household consists of a householder and one or more persons related to the householder by marriage, birth, or adoption.

## POPULATION AND HOUSING PROJECTIONS

Population and housing projections provide a view of how the Borough is expected to grow during the next 20 years. Projections are the basis for future planning for housing, park and recreation facilities, senior services, emergency services, and other community facilities. The Delaware Regional Planning Commission (DVRPC), with input from the Bucks County Planning Commission, provides population forecasts for the nine-county region that it serves. The most recent forecasts were completed in May 2012, and incorporate data gathered from the 2010 Census. The DVRPC population forecasts are based on an age-cohort component model that incorporates birth rates, death rates, survival rates, and estimates of migration rates. The forecasts are adjusted to account for local land use trends as shown in Table 4.

**Table 4. Projected Population Growth, 2010 to 2040**

								2010-2040	
	2010	2015	2020	2025	2030	2035	2040	Amount Change	Percent Change
<b>New Britain Borough</b>	3,152	3,017	3,160	3,200	3,240	3,298	3,355	203	6.4
Chalfont Borough	4,009	4,069	4,080	4,155	4,230	4,368	4,507	498	12.4
Doylestown Township	17,565	17,563	18,570	19,200	19,830	20,454	21,078	3,513	20.0
New Britain Township	11,070	11,236	11,520	11,845	12,170	12,640	13,111	2,041	18.4
Warrington Township	23,418	23,942	26,040	27,455	28,870	30,248	31,625	8,207	35.0
<b>TOTALS</b>	<b>59,214</b>	<b>59,827</b>	<b>63,370</b>	<b>65,855</b>	<b>68,340</b>	<b>71,008</b>	<b>73,676</b>	<b>14,462</b>	<b>24.4</b>
<b>Central Bucks Region</b>	247,095	249,513	262,470	270,960	279,450	287,974	296,499	49,398	20.0
<b>Bucks County</b>	625,249	627,367	654,140	673,290	692,440	709,793	727,150	101,901	16.3
<b>DVRPC Region</b>	5,626,185	5,717,932	5,777,661	5,935,259	6,098,853	6,197,417	6,261,673	635,488	11.3

*Source: U.S. Census Bureau, Delaware Valley Regional Planning Commission*

The DVRPC forecasts indicate that the population of the Borough will continue to experience a period of gradual increases over the next 25 years. During the period of 2010 to 2040 it is anticipated that New Britain Borough will add 203 residents. This rate of growth is lower than the adjacent municipalities and the municipalities within the Central Bucks Region. Both the adjacent municipalities and the Central Bucks Region are projected to grow at faster rates than both Bucks County and the DVRPC region.

In order to address future land use changes associated with future growth, projected population growth can be translated into projected housing growth. High and low housing projections were prepared by dividing the forecasted population by the average persons per dwelling unit to estimate the increase in residential units to 2040.

The low projection indicates a possibility of an additional 64 dwelling units in the Borough by the year 2040. The high projection indicates a possibility of 100 additional dwelling units in the Borough by year 2040.

Any forecast of future growth is tentative and subject to a given set of assumptions<sup>6</sup> holding true for a defined period of time and the constraints of the projection model employed. The nature of growth will be dependent on future housing markets and land use policies of the Borough. Additional residential development of 117 dwelling units has already been proposed for redevelopment of the Knoell property and plans have been approved for 16 dwelling units in the Hidden Meadows development on North Shady Retreat Road.

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<sup>6</sup> The following assumptions were used to develop the housing projection:

1. The Census 2010 amounts for population and housing units are approximately correct and take into account all units constructed up until the end of 2009.
2. The population projections of the DVRPC to 2040 are approximately correct.
3. The use of the average number of persons per dwelling unit to conduct this projection provides an accurate measure of the amount of housing that will be needed for the projected population and accurately accounts for the effect of vacancies on these projections.

## LAND USE CHARACTERISTICS

### REGIONAL LAND USE

New Britain Borough is located within the Central Bucks Region which contains rural and suburbanized areas and is the transitional area between the Upper and Lower Bucks regions. The total land area for this region is almost 150,000 acres, equating to roughly 30 percent of the County's total area. This region is considered the most geographically diverse in the County due to development patterns ranging from urban (boroughs), to suburban, to rural. It contains more land devoted to single-family residential, government and institutional, and park, recreation, and protected open space than either Upper or Lower Bucks County.

During the last 25 years, land use trends within Central Bucks are highlighted by a loss of agricultural and undeveloped lands that have resulted in increases in the categories of single-family residential, park, recreation and protected open space, and commercial uses.

### STUDY AREA LAND USE

New Britain Borough contains over 1.22 square miles (approximately 782 acres) of land. The study area contains over 255 acres or approximately one-third of the land area within the Borough. The table below provides a snapshot of land use acreages within the study area in 2015, and Map 2 shows the 2015 parcel-specific land use for the study area.

**Table 5. 2015 Study Area Land Use Characteristics**

Land Use Category	Acres	Percentage
Single-Family Residential	59.8	23.4
Multifamily Residential	9.3	3.6
Rural Residential	13.7	5.4
Manufacturing	8.6	3.3
Commercial	76.8	30.0
Government and Institutional	56.8	22.2
Park, Recreation, and Protected Open Space	20.6	8.1
Transportation and Utilities	5.3	2.1
Vacant	4.7	1.9
<b>Total</b>	<b>255.6</b>	<b>100.0</b>

### RESIDENTIAL LAND USE

Altogether, residential uses comprise the largest land use category within the corridor encompassing 32.4 percent (82.8 acres) of the land within the corridor. The residential land uses are broken down into Rural, Single-Family, and Multifamily residential uses.

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### *SINGLE-FAMILY RESIDENTIAL*

Single-Family Residential (59.8 acres or over 23 percent of the study area) is the third largest land use category within the corridor and the largest category of residential land use. Lot areas for the Single-Family Residential uses in the corridor average about half of an acre. Larger single-family lots, greater than one acre, are found on the north side of Butler Avenue between Lamp Post Road and Sandy Ridge Road amongst the rural residential lots. Small Single-Family Residential lots are found at the intersection of Butler Avenue and Tamenend Avenue and also on the south side of Butler Avenue between Shady Retreat Road and Iron Hill Road.

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### *MULTIFAMILY RESIDENTIAL*

The Multifamily Residential uses (9.3 acres or 3.6 percent of the study area) within the corridor are located in only two places. The first is the Carousel Pointe and Forest Park residential developments located at the far western end of the Borough at the northeast corner of the intersection of Butler Avenue and Bristol Road. The Forest Park development consists of 87 townhouses and Carousel Pointe contains 80 multifamily units (condominium units). Also associated with these developments are a total of 20.6 acres of common space that is included under the Parks, Recreation and Open Space category.

The other Multifamily Residential development is the Cedar Woods Townhouse development located at the northwest corner of Butler Avenue and Cedar Drive. This development consists of nine attached townhomes and one duplex unit for a total of 11 dwelling units on the 1.64 acre parcel.

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### *RURAL RESIDENTIAL*

Parcels within the Rural Residential category are greater than 5 acres and include residential dwellings. The Rural Residential land use within the corridor includes two adjacent parcels, with a combined total of 13.7 acres or 5.4 percent of the study area, located on the north side of Butler Avenue between Lamp Post Road and Sandy Ridge Road.

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## NONRESIDENTIAL LAND USE

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### *COMMERCIAL*

Commercial land use is the largest nonresidential land use category with 76.8 acres or 30.0 percent of the study area. The commercial land use includes 76.8 out of 90.2 acres (85.1 percent) of the commercial land use within the Borough. Commercial uses are concentrated at the western and eastern ends of the Borough with scattered uses along both sides of Butler Avenue. At the western end of the Borough is the Town Center of New Britain Shopping Center and Office Park. The shopping center contains 124,000 square feet of retail uses and 626 parking spaces. The Giant Food Stores supermarket is the anchor tenant and largest use by square footage within the shopping center. Other uses include a variety of retail, restaurants, and personal services. The five buildings in the office park a total of 29 offices and an approximately 38,000 square feet of office space. The tenants in the office park include medical offices, a child care provider, and business offices.

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## MANUFACTURING

Manufacturing use includes two parcels with a total area of 8.6 acres or 3.3 percent of the study area, one of which is currently operating in a manufacturing capacity and the other is idle. Located at the southwest corner of Butler Avenue and Sand Road is the Carleton Helical Technologies manufacturing facility on 1.4 acres. This business manufactures equipment for the packaging industry. The other parcel within the manufacturing category is the site of the former Knoell woodworking facility at the northeast intersection of Butler Avenue and North Shady Retreat Road.

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## GOVERNMENT AND INSTITUTIONAL

Government and Institutional land uses within the study area comprise 56.8 acres or 22.2 percent of the study area and include a portion of the Delaware Valley University (DVU) campus, two places of worship, a post office, an elementary school, and a psychiatric center. The largest Government and Institutional land use is the 21.3 acre portion of the 570 acre DVU campus. The portion of the campus within the Borough includes academic buildings, residential buildings, and athletic facilities.

The places of worship include the Lenape Valley Presbyterian Church at the northwest corner of Butler Avenue and Ute Road, and the New Britain Baptist Church at the southeast corner of Butler Avenue and South Tamenend Avenue.

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## PARK, RECREATION, AND PROTECTED OPEN SPACE

Park, Recreation, and Protected Open Space accounts for 20.6 acres (8.1 percent of the study area). Land use in this category is limited to the common space of the Forest Park and Carousel Pointe residential developments located at the northeast intersection of Butler Avenue and Bristol Road.

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## TRANSPORTATION AND UTILITIES

The Transportation and Utilities land use consists of 5.3 acres or 2.1 percent of the study area and includes 3.6 acres of SEPTA right-of-way, water utility, and electric utility substations at the western end of the study area.

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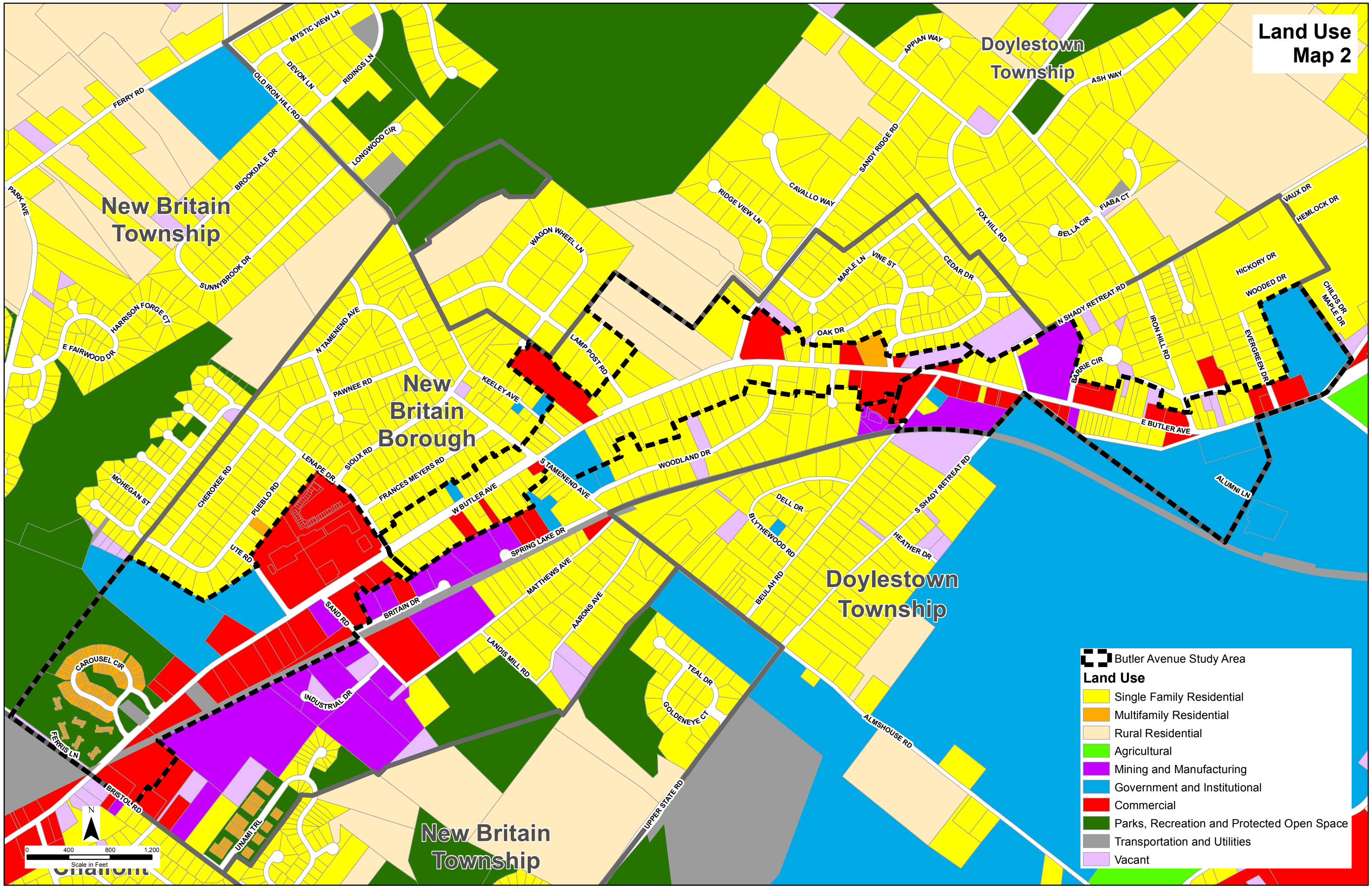
## VACANT

Vacant land includes parcels without dwelling units and account for 4.7 acres or 1.9 percent of the study area. The largest parcel within this category is a 2.1 acre parcel on the north side of Butler Avenue. Environmental constraints, including the presence of Cooks Run and steep slopes on the parcel, will limit its development. The Borough owns a vacant parcel along what would be the right-of-way of the Bristol Road extension. The other vacant parcels appear to have potential for development either as residential uses or small-scale commercial uses.

## HISTORIC RESOURCES

Settlement of the area which would become New Britain Borough began in the early 1700s. Examples of historic architecture include buildings from the Colonial, Revolutionary, Federal, and Victorian periods. The historic resources that remain are standing reminders of the ways and values of the people who settled the area and provide a physical and cultural context to the appearance and function of the corridor. The Borough's Historic Preservation Commission has identified 129 structures as having historic significance to the community. These properties are shown on Map 3 and a listing of the properties are included in Appendix I.

Land Use  
Map 2



Historic Resources  
Map 3



New Britain  
Township

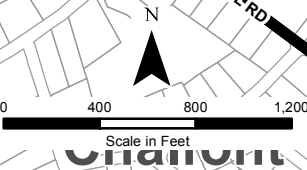
Doylestown  
Township

New  
Britain  
Borough

Doylestown  
Township

New Britain  
Township

Historic Resources



## UNDERUTILIZED PROPERTIES

The Borough's revitalization efforts within the study area may take place in existing structures, thorough adaptive reuse or conversion, or on vacant land where available. Therefore, the identification of underutilized properties is necessary to the future success of revitalization efforts so that development is guided to appropriate locations. The following sites, shown on Map 4, have been identified as underutilized and potentially suitable for redevelopment due to current vacancy status, business relocation/closure, or site characteristics. A site's present use(s) or site layout may also not lend itself to optimizing the characteristics and potential success of the Borough's revitalization vision. These underutilized properties have also been identified as being part of the Opportunity Areas shown in the segment plans in the Land Use Plan chapter. A brief description of the properties is found below.

### **Knoell Property (TMP #25-9-2)**

The Knoell Property (TMP #25-9-2) is located on the north side of East Butler Avenue between Shady Retreat Road and Barrie Court and is comprised of approximately 7.1 acres. The site is within the LI Light Industrial District and the Mixed Use Overlay. The Borough has received preliminary plans for the redevelopment of the site to include 117 residential units and 21,000 square feet of commercial space. This development is the first to use the provisions of the Mixed Use Overlay Ordinance.



### **Bitzer's Cleaners (542 East Butler Avenue, TMP #25-9-38)**

The former Bitzer Cleaners property (TMP #25-9-38) is located adjacent to the Duck Deli property on the south side of East Butler Avenue. The site is zoned C-2 Commercial and is within the Mixed Use Overlay District. The property 0.37-acre parcel is currently vacant but has been used as a tailor/dry cleaner/furrier business in the past. This property, along with the adjacent Duck Deli property, has recently been purchased by a real estate developer.



**Duck Deli (524 East Butler Avenue, TMP #25-9-37)**

The former Duck Deli restaurant is located on the south side of East Butler Avenue. The site is zoned C-2 Commercial and is within the Mixed Use Overlay District. The 0.30 acre site includes a 2,600-square foot building that is not occupied at present. Together with 542 East Butler Avenue (Bitzer's Cleaners site) this site could be redeveloped at a higher intensity. As mentioned above, this property has recently been purchased by a real estate developer.



**Smith Marine/Alley-Gator Shop (573 East Butler Avenue, TMP#s 25-9-19 and -20)**

Located adjacent to the Knoell property on the north side of Butler Avenue is the 1.97-acre Smith Marine/Alley-Gator Shop property (TMP#s 25-9-19 and -20). The present uses on the property include sales and service of all-terrain vehicles, boats, and personal watercraft and automotive repair. The site is zoned C-2 Commercial and is within the MUO Mixed Use Overlay. The size and location of the property make it an attractive site for redevelopment.



**Delaware Valley Concrete (20 South Shady Retreat Road, TMP #25-8-3-1)**

Located behind the row of commercial properties that front the south side of Butler Avenue between Beulah Road and South Shady Retreat Road is the Delaware Valley Concrete ready mix concrete plant. The 4.67-acre property is zoned LI Light Industrial and located within the MUO Mixed Use Overlay. The size and location of the property make it an attractive site for redevelopment.



### **Town Center of New Britain Shopping Center**

The Town Center of New Britain Shopping Center is a 124,000 square-foot shopping center located on 15 acres. The site is zoned SC Shopping Center. There are 24 businesses located in the two main buildings of the center and three building pads separate from these buildings. In the southwest corner of the site is a 0.9 acre open space area. The presence of this open space and an opportunity to enhance it for further public use is the rationale for including this site as underutilized.



### **341 and 401 West Butler Avenue (TMP #25-1-4 and 25-1-3-1)**

Located on either side of the Pine Run Elementary School are two properties of approximately 2 acres each. Both properties are within the C-2 Commercial District. The 2.31 acre site at 341 West Butler Avenue (TMP #25-1-4) located between the Lenape Valley Church and the Pine Run Elementary School contains the New Britain Veterinary Clinic. The 1.84-acre site at 401 West Butler Avenue (TMP #25-1-3-1) contains the Fretz Pool Company.



341 West Butler Avenue

### **New Britain Inn (376 West Butler Avenue, TMP #25-1-12)**

The New Britain Inn property is located on the south side of West Butler Avenue. The 2.47-acre site is zoned C-1 Commercial. A tavern is located on the site with parking located in the front and side yards. The size of the site gives it the potential to be further developed.



# Underutilized Properties Map 4



## TRANSPORTATION AND CIRCULATION

Community transportation issues involve more than just moving vehicles safely and efficiently. To fully understand transportation problems and solutions, economic development, public safety, the environment and quality of life for residents must be considered. Integrated transportation, land use, and circulation systems that are well-designed help preserve a sense of community, advance economic development goals, and preserve both public and private infrastructure investments. Transportation is a primary quality of life issue for all those who work, reside or travel through New Britain Borough.

This section of the study will examine the road system of New Britain Borough, review recommendations from previous transportation studies, evaluate the need for new road and intersection designs, provide access management recommendations and suggest potential funding scenarios for proposed transportation-related projects.

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### TRANSPORTATION SYSTEM

New Britain Borough contains several major roads. They include the Butler Avenue (U.S. Business Route 202), Bristol Road and Tamenend Avenue. Butler Avenue is classified as a Principal Arterial that runs south to north through the study area. The majority of Butler Avenue is one lane in each direction, with supplemental turning lanes at some intersections. Bristol Road is classified as a Minor Arterial that runs west to east on the edge of the study area. Bristol Road is one lane in each direction. Tamenend Avenue is classified as an Urban Collector and runs east from Butler Pike eventually becoming Almshouse Road east of Upper State Road. Tamenend Avenue has a two-lane cross section.

Butler Avenue was formerly U.S. Route 202, but that designation was removed when the U.S. Route 202 Parkway was constructed in 2012. Based upon concerns from business owners, Bucks County, on behalf of the municipalities along Butler Avenue, pursued designating Butler Avenue from Route 611 in Doylestown Township to Route 309 in Montgomery Township as U.S. Business Route 202. Creating a “business route” has been shown to help improve or maintain commerce and industry for businesses that abut and are in the vicinity of a “Business Route.”

This is particularly important for this portion of Butler Avenue in Chalfont Borough, New Britain Borough, New Britain Township and Doylestown Township, since it functions as a “Main Street” in the boroughs and townships. For decades prior to construction of the U.S. Route 202 Parkway, the businesses located along this road had a Route 202 mailing address. Both Bucks and Montgomery counties and all of the municipalities located along Butler Avenue agree that designating Butler Avenue/Doylestown Pike to Business Route 202 will have a positive effect on the businesses located in the corridor. The designation of Butler Avenue as U.S. Business Route 202 was granted by the American Association of Transportation Officials (AASHTO) in 2015. PennDOT has recently completed adding signage to delineate the route.



## TRAFFIC COUNTS

Traffic through this corridor is moderately heavy, according to PennDOT's Internet Traffic Monitoring System, Butler Avenue had an average daily traffic (ADT) of 11,928 vehicles in 2015 in the segment from Bristol Road to South Tamenend Avenue, 10,801 vehicles in 2013 in the segment from South Tamenend Road to Sandy Ridge Road and 10,962 vehicles in 2015 in the segment from Sandy Ridge Road to New Britain Road. Trucks accounted for 4 to 8 percent of this volume. Traffic volumes for all sections of roadways in the study area are shown in Table 6.

**Table 6. Traffic Volumes**

<b>Butler Avenue (U.S. Business Route 202)</b>				
<b>From</b>	<b>To</b>	<b>ADT</b>	<b>Year</b>	<b>% Trucks</b>
Bristol Road	South Tamenend Avenue	11,928	2015	8%
South Tamenend Avenue	Sandy Ridge Road	10,801	2013	4%
Sandy Ridge Road	New Britain Road	10,962	2015	5%
<b>South Tamenend Avenue</b>				
<b>From</b>	<b>To</b>	<b>ADT</b>	<b>Year</b>	<b>% Trucks</b>
Butler Avenue	Upper State Road	2,158	2015	5%
<b>South Shady Retreat Road</b>				
<b>From</b>	<b>To</b>	<b>ADT</b>	<b>Year</b>	<b>% Trucks</b>
Butler Avenue	Upper State Road	2,898	2014	5%
<b>Bristol Road</b>				
<b>From</b>	<b>To</b>	<b>ADT</b>	<b>Year</b>	<b>% Trucks</b>
Butler Avenue	Upper State Road	5,888	2013	4%
<b>Sandy Ridge Road</b>				
<b>From</b>	<b>To</b>	<b>ADT</b>	<b>Year</b>	<b>% Trucks</b>
Butler Avenue	Iron Hill Road	1,770	2013	2%
<b>Keeley Avenue</b>				
<b>From</b>	<b>To</b>	<b>ADT</b>	<b>Year</b>	<b>% Trucks</b>
Butler Avenue	Ferry Road	1,774	2013	5%

*Source: PennDOT's Internet Traffic Monitoring System*

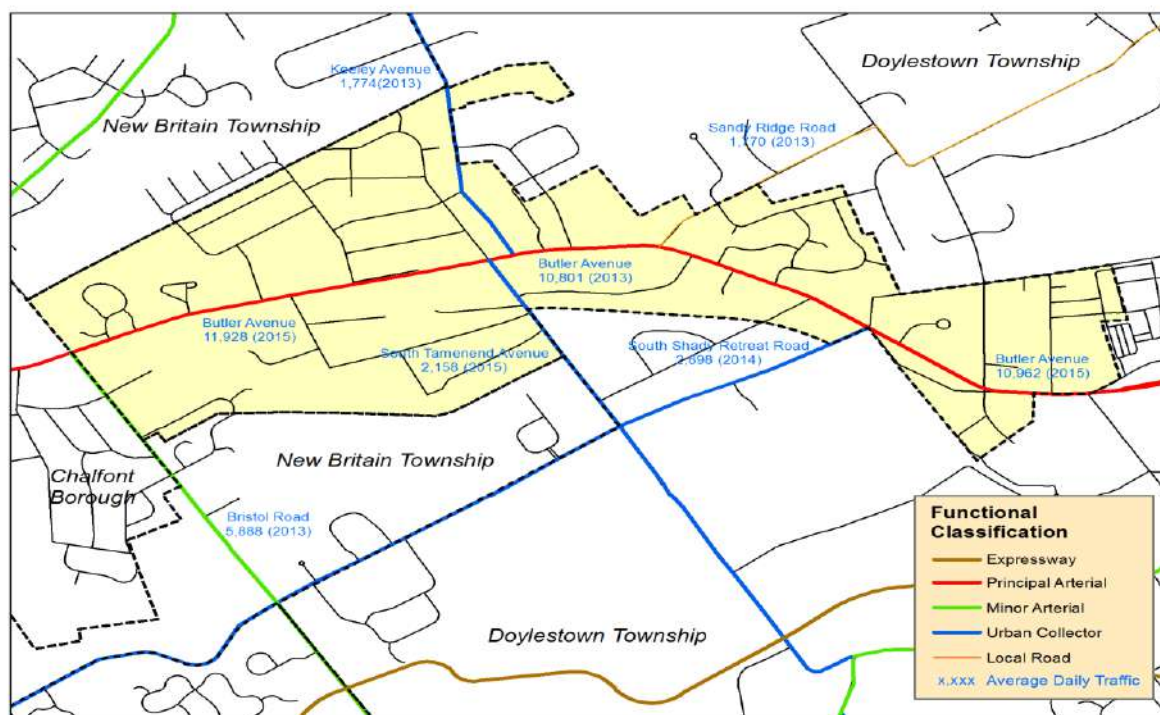
Due to the construction of the U.S. Route 202 Parkway in 2012, traffic volumes along Butler Avenue have decreased. According to PennDOT's Internet Traffic Monitoring System, traffic along Butler Avenue decreased from 12,241 vehicles in 2012 to 11,928 vehicles in 2015 in the segment from Bristol Road to South Tamenend Avenue. This represents a decrease of 313 vehicles. Traffic volumes also decreased from 19,685 ADT in 2012 to 10,962 vehicles in 2015 in the segment from South Tamenend Road to New Britain Road. This represents a decrease of 8,723 vehicles. It is unclear why such a wide variation occurred in the decreases in traffic volumes. The collection of traffic volume data is impacted by several factors, including potential nearby detours, special events and other factors unknown at the time of volume collection. It is clear that construction of the U.S. Route 202 Parkway has had a positive impact on Butler Avenue through a reduction in congestion since there are fewer vehicles on Butler Avenue.

**Table 7. Pre- and Post- U.S. Route 202 Parkway Construction**

<b>Butler Avenue (U.S. Business Route 202)</b>				
<b>From</b>	<b>To</b>	<b>2012 ADT</b>	<b>2015 ADT</b>	<b>Change</b>
Bristol Road	South Tamenend Avenue	12,241	11,928	-313
South Tamenend Avenue	Sandy Ridge Road	19,685	10,962	-8,723

*Source: PennDOT's Internet Traffic Monitoring System*

**Figure 2: Functional Classification and Traffic Counts**



*Source: PennDOT*

## TRAFFIC SPEEDS

Throughout the plan process, residents and business owners have expressed concerns about the high rate of speed for vehicles traveling along Butler Avenue. The posted speed limit within the study area is 40 miles per hour. In 2014, the Central Bucks Regional Police Department conducted a traffic study that included speed measurements that recorded the 85th percentile speed<sup>7</sup> for vehicles traveling both eastbound and westbound. The highest recorded 85th percentile speed of 48 miles per hour was recorded for westbound vehicles at 162 East Butler Avenue which is located between Lamp Post Road and Sandy Ridge Road. The highest eastbound 85th percentile speed was 45 miles per hour recorded at 125 West Butler Avenue which is located between Lenape Drive and Tamenend Avenue.

<sup>7</sup> The 85th percentile speed is defined as the speed at or below which 85 percent of all vehicles are observed to travel under free flowing conditions past a nominated point.

**Table 8. 2014 Traffic Study**

		<b>85% Percentile Speed</b>	<b>Average Daily Volume</b>
<b>470 W. Butler Ave</b>			
	Eastbound	42 mph	6,404
	Westbound	39 mph	6,114
<b>380 W. Butler Ave</b>			
	Eastbound	44 mph	6,745
	Westbound	44 mph	6,550
<b>276 W. Butler Ave</b>			
	Eastbound	39 mph	5,652
	Westbound	42 mph	6,030
<b>194 W. Butler Ave</b>			
	Eastbound	42 mph	5,312
	Westbound	41 mph	5,833
<b>125 W. Butler Ave</b>			
	Eastbound	45 mph	5,920
	Westbound	47 mph	6,371
<b>38 W. Butler Ave</b>			
	Eastbound	42 mph	5,920
	Westbound	42 mph	5,274
<b>15 E. Butler Ave</b>			
	Eastbound	43 mph	5,293
	Westbound	42 mph	5,237
<b>53 E. Butler Ave</b>			
	Eastbound	43 mph	5,293
	Westbound	42 mph	5,237
<b>162 E. Butler Ave</b>			
	Eastbound	44 mph	5,048
	Westbound	48 mph	5,718
<b>E. Butler Ave at Heritage</b>			
	Eastbound	44 mph	5,412
	Westbound	44 mph	5,119
<b>450 E. Butler Ave</b>			
	Eastbound	44 mph	4,905
	Westbound	43 mph	4,923
<b>571 E. Butler Ave</b>			
	Eastbound	44 mph	6,699
	Westbound	45 mph	5,921

The study confirms that most of the traffic is traveling above the posted 40 miles per hour speed limit.

#### *ACCESS ISSUES*

The study area contains numerous nonresidential uses with frontage along Butler Avenue that have no clear or formal access points. The identified parcels include TMPs (tax map parcels) #25-1-12, 25-2-37, 25-6-18-4, 25-6-57, 25-9-38, 25-9-39, 25-9-19, and 25-9-77 and are shown in the corresponding pictures. These parcels could benefit from the implementation of access management techniques.

### Potential Access Management Locations



*TMP# 25-001-012*



*TMP# 25-002-037*



*TMP# 25-006-018-004*



*TMP# 25-009-019*



*TMP# 25-009-038 & 39*



*TMP# 25-009-077*



*TMP# 25-006-057*

Access management can be addressed on a site-by-site basis as a property becomes redeveloped or if property owners decide to make these improvements on their own accord.

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### THE BUTLER AVENUE/SHADY RETREAT ROAD INTERSECTION

The Butler Avenue/Shady Retreat Road intersection has been identified as a key intersection for this study. This intersection suffers from poor alignment in that both legs of Shady Retreat Road do not intersect with Butler Avenue at 90 degree angles. The road that provides access to several businesses and multiple residences along Butler Avenue creates a “fifth leg” at the intersection. The driveways for the 7-Eleven at the intersection create additional turning movements, which lead to additional conflict points at an already problematic intersection.



*Butler Avenue/Shady Retreat intersection*

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### PEDESTRIANS AND BICYCLISTS

A pedestrian- and bike-friendly town is typically associated with a high quality of life and a sense of community. In many communities, pedestrian and bicycle systems are important and much appreciated facilities. These facilities can have a substantial impact on local traffic and congestion and have a positive environmental impact.

Along Butler Avenue there are sidewalks in a limited number of areas. Sidewalks are located on the north side of West Butler Avenue from Bristol Road to Pine Run Elementary School, construction completed in 2016, and on the north side of West Butler Avenue along the two service roads, Access A and Access B, between Lenape Drive and North Tamenend Avenue. A length of narrow sidewalk exists along East Butler Avenue from the corner of the intersection with North Tamenend Avenue. Sidewalks on the south side of West Butler Avenue are limited to the frontage of the Children of America child care center at 386 West Butler Avenue (TMP #25-1-11-1), the adjacent First Savings Bank of Perkasio at 380 West Butler Avenue (TMP#25-1-11), the Farmhouse Professional Center at 314 West Butler Avenue (TMP #25-1-14), and the New Britain Baptist Church at 22 East Butler Avenue (TMP#25-2-48).

Pedestrian crossings of Butler Avenue are limited to the signalized intersections at Sand Road, Iron Hill Road, and New Britain Road. Crosswalks and pedestrian crossing signals are present at these intersections. However, curb ramps for handicap accessibility are only present at the New Britain Road intersection. Pedestrian crossing is restricted at the intersections with Bristol Road and Tamenend Avenue through the use of signage.

There are no bicycle facilities within the study area. Outside of the study area the Destination Peace Valley Bike and Hike trail passes through Covered Bridge Park and informal grass trails are mowed at Wilma Quinlan Nature Preserve.

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## PUBLIC TRANSPORTATION

The Southeastern Pennsylvania Transportation Authority (SEPTA) provides public transportation service linking Philadelphia and the surrounding counties with an integrated network of over 200 bus and rail transit routes. SEPTA's Regional Rail Division operates seven electrified commuter rail routes to over 150 stations in Pennsylvania, New Jersey, and Delaware. SEPTA also operates an extensive network of buses, trolleys, and rapid transit trains in Center City Philadelphia, in addition to Regional Rail links to downtown employment centers, the University City area of West Philadelphia, and the Philadelphia International Airport.

The study area is directly served by SEPTA's Lansdale/Doylestown Regional Rail Line at the New Britain Station and the Delaware Valley University Station. The New Britain station is located along South Tamenend Avenue, approximately 900 feet east of Butler Avenue, at Matthews Avenue at the New Britain Borough/Doylestown Township line. New Britain Station is the second station stop in Bucks County for the Lansdale/Doylestown Regional Rail Line.

This station is 1.3 miles south of Delaware Valley University Station and 1.8 miles north of Chalfont Station. This station is within walking distance of a handful of residences, and its primary function is that of a small-scale “park-and-ride” style facility for those who drive from other locations. In 2006, improvements were completed that included the addition of high-level accessible platforms and a new passenger shelter. The station is used mostly by commuters, as evidenced by the station's flag stop only service on weekends. Full service is maintained on weekdays.

Figure 3: SEPTA Lansdale/Doylestown Regional Rail Line, New Britain Station



#### New Britain Station Profile

**Address:** Tamenend Avenue and Matthews Avenue

**Parking:** 39 Spaces, 62% filled to capacity

**ADA Spaces:** 2 Spaces

**AM Peak-hour time to Jefferson Station:** 62 to 70 minutes

**AM Peak-hour headway:** 24 to 51 minutes

**PM Peak-hour time from Jefferson Station:** 69 to 82 minutes

**PM Peak-hour headway:** 31 to 44 minutes

**Fare Zone:** 4

**Ticket Office:** None

**Connecting Service:** None

**Daily Weekday Boards (2013):** 51

**Daily Weekday Leaves (2013):** 58

The study area is also served by SEPTA's Lansdale/Doylestown Regional Rail Line through the Delaware Valley University Station. It is located on the Delaware Valley University campus at the rear of the student parking lot and adjacent to James Work Memorial Stadium and is the last station stop before the Lansdale/Doylestown Regional Rail Line terminates at Doylestown Station. The station stop has previously been flagged for discontinuation by SEPTA because its use falls below performance standards. However, SEPTA has agreed to keep the station stop in operation because Delaware Valley University has agreed to maintain the station facilities.

While regulations require that this station be available to the general public, it is generally regarded as an amenity for students, faculty, and visitors of Delaware Valley University.

**Figure 4: SEPTA Lansdale/Doylestown Regional Rail Line, Delaware Valley University Station**



Delaware Valley University Station Profile

**Address:** Delaware Valley University Campus, 700 East Butler Avenue

**Parking:** No designated public spaces

**ADA Spaces:** 4 Spaces

**AM Peak-hour time to Jefferson Station:** 65 to 72 minutes

**AM Peak-hour headway:** 24 to 52 minutes

**PM Peak-hour time from Jefferson Station:** 71 to 84 minutes

**PM Peak-hour headway:** 31 to 44 minutes

**Fare Zone:** 4

**Ticket Office:** None

**Connecting Service:** None

**Daily Weekday Boards (2013):** 68

**Daily Weekday Leaves (2013):** 70

In 2013, Bucks County Planning Commission, with assistance from the TMA of Bucks County, completed the *Bucks County Regional Rail Station Visibility Enhancement Analysis for Lansdale/Doylestown Regional Rail Line & West Trenton Regional Rail Report*. This study provided specific recommendations for the rail stations along SEPTA's regional rail network that were designed to enhance and promote additional ridership on the SEPTA public transportation system.

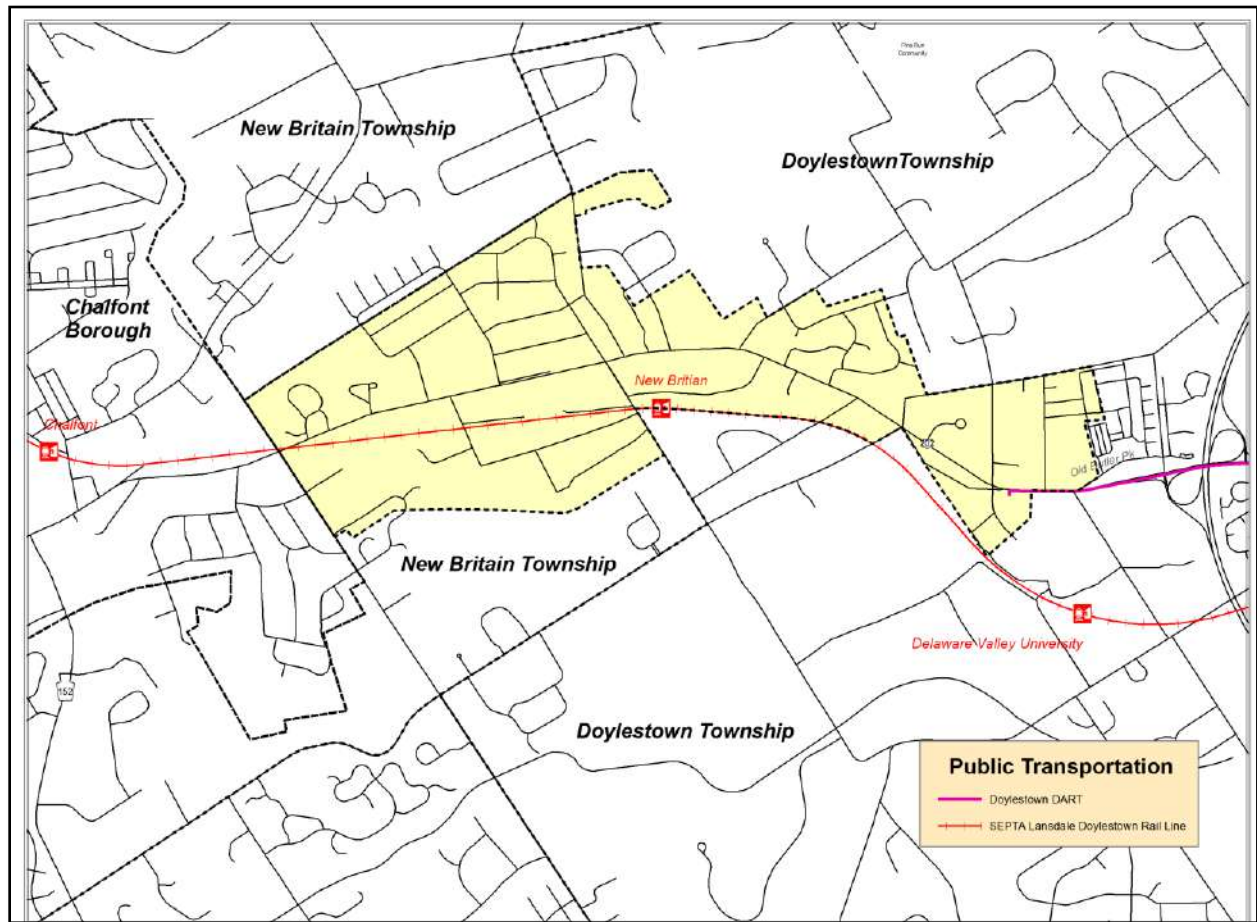
Since Delaware Valley University Station is basically an amenity for the university's students, no specific recommendations were made for the station. However, the report did note that the New Britain Station had a lack of sidewalks and minimal shoulders along South Tamenend Avenue which prevents safe passage to the station by pedestrians from Butler Avenue. Fortunately, this condition is being addressed by New Britain Borough through a combination of transportation grants. Once completed, this pedestrian improvement project will include the construction of an 8 to 10-foot wide shared use path along South Tamenend Avenue from Butler Avenue to the train station. The project also includes the addition of pedestrian push buttons at the South Tamenend Avenue and Butler Avenue intersection. Improvements to the existing railroad crossing area for the shared use path are also included in the project. These improvements will connect the large residential development on the west side of Butler Avenue to the New Britain train station.



The Station Visibility report also recommended improved signage alerting motorists and pedestrians to the train station's location. While the station is well marked and plainly visible to motorists traveling on South Tamenend Avenue, visitors who are unfamiliar with the area would encounter difficulty in locating this station. Recommended signage locations in the study included the Butler Avenue/South Tamenend intersection and the South Tamenend Avenue intersection with Upper State Road in Doylestown Township.

The study area is also served by the Doylestown DART, which is a public transportation service consisting of small buses operated by Bucks County Transport. The DART consists of small buses which provide headways of approximately 1.5 hours and connects to various commercial, residential, government offices and hospitals in the Doylestown area. This service also provides connections to SEPTA's Route 55 bus and the Lansdale/Doylestown Regional Rail Station in Doylestown Borough. This service provides a connection to the Colonial Heritage Mobile Home Park and Delaware Valley University. The borough should continue efforts to analyze how potential extension of this bus service could benefit borough residents and businesses.

Figure 5: Public Transportation

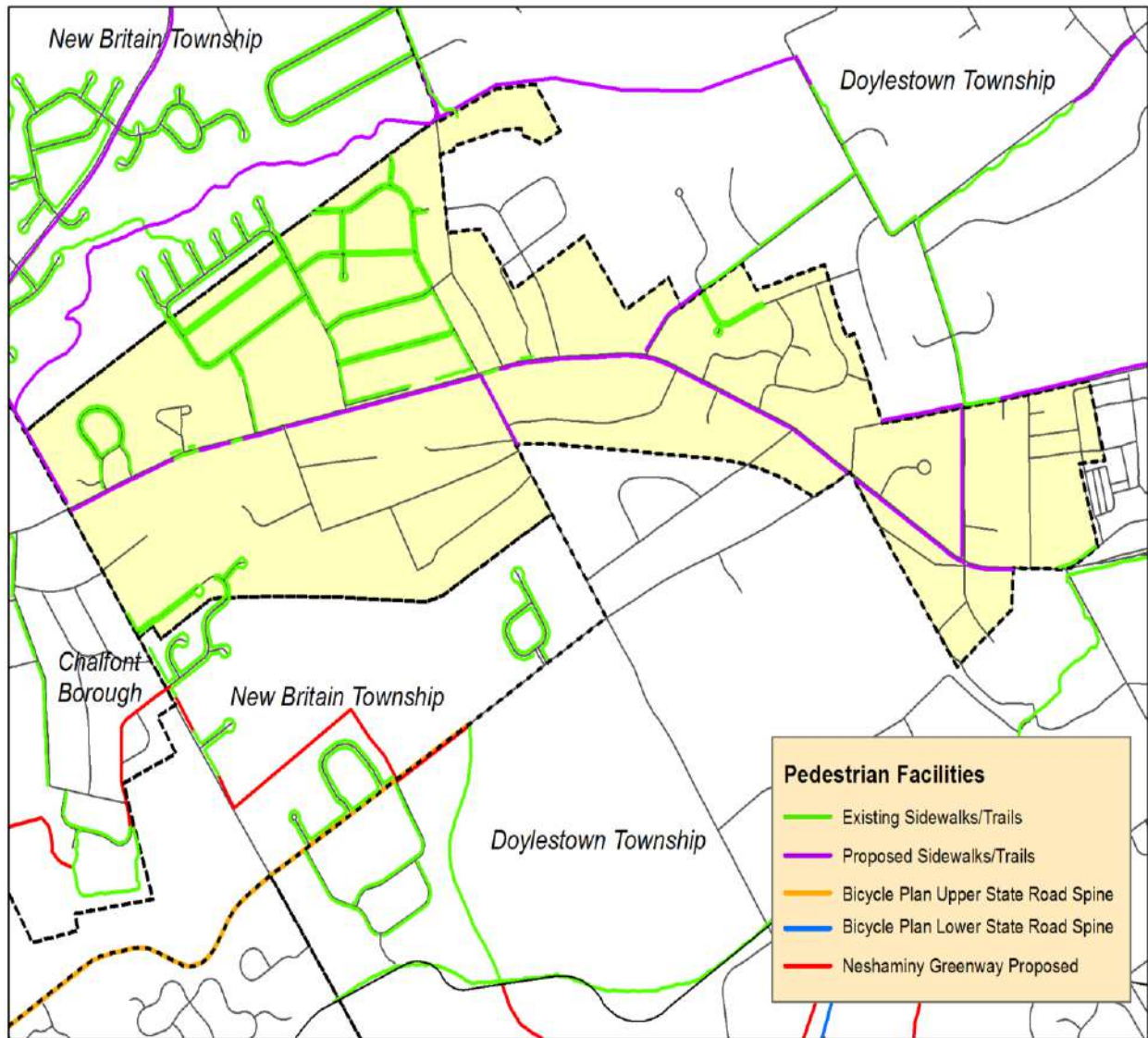


## DVRPC TRANSPORTATION IMPROVEMENT PROGRAM

DVRPC and its member governments prepare a program of projects that responds to the needs of the region and at the same time complies with federal and state policies. As of the *FY 2015 Transportation Improvement Program for Pennsylvania*<sup>8</sup>, there is only one project slated for construction within the study area, the Bristol Road Extension. This project includes a two-lane extension of Bristol Road (approximately 2,000 feet) from Butler Avenue to Park Avenue. When completed, this improvement will provide a two-lane bypass around Chalfont Borough. The goal of the project is to eliminate trips on Butler Avenue and minimize turning movements at the Butler Avenue/Route 152 intersection. The project involves relocation of a SEPTA siding track, a bridge across the wetlands, and widening of the intersection at Bristol Road and Butler Avenue to provide right and left turning lanes. This project is slated to begin construction in 2025.

<sup>8</sup> The Delaware Valley Regional Planning Commission (DVRPC) produced the *FY 2015 Transportation Improvement Program (TIP) for Pennsylvania* for Bucks, Montgomery, Chester, Delaware and Philadelphia counties.

Figure 6: Pedestrian and Bike Facilities



## POLICY AND REGULATORY PROVISIONS

An analysis of the planning documents relevant to the study area was undertaken to ensure that, where applicable, this plan would fit with the existing policies and regulations and determine which policies and regulations may inhibit development and revitalization of the area. A brief description of each document and the major recommendations that would affect the corridor have been included below.

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### NEW BRITAIN BOROUGH COMPREHENSIVE PLAN

The *New Britain Borough Comprehensive Plan* (2007) has been used to shape the Borough's land use policies and regulations. The plan addresses community planning issues such as housing, parks and open space, water supply and wastewater disposal, historical and cultural resources, natural resources, nonresidential development and economic activity, and transportation and circulation. The comprehensive plan places special emphasis on several topics, including the Butler Avenue corridor, the protection of historic resources, water supply and sewerage systems, and the provision of public services.

The Housing and Residential Areas chapter examines the residential land use within the corridor and provides policies for the future residential use of land. The overall goal for residential land use is to preserve the residential character of the Borough and accommodate a variety of housing types.

The plan identifies the areas between the intersections of Butler Avenue and Heritage Lane, Butler Avenue and Lenape Drive, and the eastern end of the north side of Butler Avenue closest to Doylestown Township as "Stable Residential Areas." These areas are dominated by residential uses and stable residential neighborhoods with few intrusions from nonresidential uses. The land use policies for these areas include:

- Retain the residential character of Butler Avenue in the identified areas.
- Avoid changes of zoning from residential to nonresidential uses, except where compatible uses might provide good buffers for residential areas.
- Continue to allow for home occupations and home offices in these areas.

The plan also identifies "Mixed Areas along Route 202 (Butler Avenue)" as areas where residences are located in the midst of or across the street from commercial activities and are affected by the nearby nonresidential uses due to traffic, commercial lighting, and signs. These areas are located primarily at the eastern end of the Borough, from Beulah Road to Doylestown Township and are within the C-2 Commercial and C-3 Office Commercial zoning districts. The land use policies for these areas include:

- Maintain the C-2 Commercial and C-3 Office Commercial zoning districts allowing for the natural conversion of residential uses to nonresidential uses when economically feasible.
- Carefully planned landscaping and control of lighting and signage should be enforced to limit impacts to adjacent residential properties.

Analysis of the commercial areas is undertaken in the Nonresidential Development and Economic Activity chapter. The overall goal is to support and encourage the improvement of existing commercial areas; allow for growth of commercial uses only in areas where they complement existing commercial developments and do not detract from residential neighborhoods.

Within this chapter, the Butler Avenue corridor was divided into four segments. Area 1 comprises the corridor from Bristol Road to Lenape Road, Area 2 from Lenape Road to Beulah Road, Area 3 from Beulah Road to Shady Retreat Road, and Area 4 from Shady Retreat Road to the end of the Borough. Each area was examined and policies developed.

Area 1 is mostly commercial with the Town Center of New Britain shopping center and the individual commercial uses on the opposite side of Butler Avenue. (For reference Area 1 coincides with the Town Center Area delineated in the *Butler Avenue Main Street Study*.) The comprehensive plan anticipates that little change would occur within Area 1; therefore, no substantial changes to the land use composition were recommended. The recommendations include:

- Require that all applications for change in use or expansion of uses go through the procedures to obtain land development and zoning approval so that the Borough can review what is proposed and have a record of what is proposed.
- Require that all necessary improvements, such as buffering, landscaping, etc., be provided as a condition of the granting of approval for a change in use.

Area 2 is primarily residential, with only a few offices located in the area. (For reference, Area 2 coincides with the Historic Village area delineated in the *Butler Avenue Main Street Study*.) Several historical buildings, including residences and the New Britain Baptist Church, are within the area. Recommendations include:

- Preservation of the historic area at the intersection of Tamenend Avenue and Butler Avenue.
- Consider development of design guidelines for the area.

Area 3 has commercial uses on the south side but is primarily residential on the opposite side. (For reference, Area 3 contains the western portion of the University Village area from Beulah Road to Shady Retreat Road as delineated in the *Butler Avenue Main Street Study*.) The plan makes the following recommendations for this area:

- Limit the permitted commercial activities so that service stations, auto repair and car washes, pharmacies, and drive-in eating places are not permitted.
- Apartments in conjunction with commercial and office uses should be permitted.
- Explore the possibility of providing for new uses in the C-3 Commercial District to encourage the reuse of existing residential buildings. Add other zoning standards, such as maximum building size and rear parking requirements. Reduce the front yard setback requirements of the district.

Area 4 is comprised of a mix of commercial and residential uses, some offices, and the campus of Delaware Valley University. (For reference, Area 4 contains the remaining University Village area as delineated in the *Butler Avenue Main Street Study*.) The plan identified the following problems within the area:

- Inadequate parking for some uses;
- No opportunities for pedestrian circulation;
- A concern about aesthetics;
- A general desire to upgrade some of the commercial uses;
- The area does not function as a cohesive area; people do not stop and patronize several places in one trip.

Recommended policies include:

- Limit types of commercial uses in this area by continuing the restricted commercial zoning designation. More intense commercial uses, such as service stations, car washes, pharmacies, convenience stores, and drive-in restaurants, should be prohibited in this area. The existing uses can remain, but new uses which would have a detrimental effect on the residences should not be permitted.
- Where owners plan to expand a use or change a use, the borough should require that the required buffering and landscaping be provided as a condition of granting approval.
- Where changes or expansion are proposed, the borough should require that parking be located to the rear or side to improve the appearance of Butler Avenue.
- The zoning ordinance should permit apartments as part of individual commercial structures.
- TMP #25-9-78, which holds a small motel, is split-zoned between R-1 Residential and C-2 Commercial. The borough should rezone the R-1 portion of the parcel to C-2 to eliminate this split zoning.

The comprehensive plan also focuses on the Butler Avenue corridor and sets forth goals in the Corridor Planning and Development section of the plan. The overall goal is to encourage the creation of a pedestrian-friendly corridor along Butler Avenue rather than a collection of highway-oriented commercial uses. The plan recommends the following:

- Plan for shared access, coordinated lights, signage, landscaping, and other improvements along the Butler Avenue corridor.
- Consider limiting parking areas to the sides and rears of buildings to help further reduce visual clutter and take advantage of the shorter building setbacks commonly found on lots along the Butler Avenue corridor.
- Reinforce a local identity and sense of place by creating gateways along Butler Avenue at the entrances of the borough.

The stated policies and recommendations found in the *New Britain Borough Comprehensive Plan* (2007) are consistent with the vision and identified goals of this study. The Comprehensive Plan's goals for Butler Avenue include creating a pedestrian friendly corridor, establishing a "Main Street" streetscape, prohibiting auto-oriented commercial development. These concepts have served as the foundation for the Butler Avenue Main Street Study.

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## BUTLER AVENUE REVITALIZATION STRATEGIES

In response to the construction of the US 202 Parkway, New Britain Borough along with Chalfont Borough and New Britain Township joined together to plan for the Butler Avenue corridor. The resultant study, *Butler Avenue Revitalization Strategies* (2009), included planning, visioning, and public outreach exercises to develop a conceptual streetscape and circulation plan for the corridor. Placemaking and transportation management techniques and improvements were identified to create a safer and more vibrant corridor, and also support its transportation needs.

The study included a final conceptual plan, included in Appendix 2, which illustrates the recommend solutions for sidewalk extensions, access management, parking management, landscape treatments, and gateway treatments. Among the study's recommendations for the Borough are:

- Construction of the missing sidewalk segments along the north side of Butler Avenue from Bristol Road to Tamenend Avenue.
- Where expansive open frontages are present along the corridor add planting edge.
- Gateway feature at intersection of Butler Avenue and New Britain Road.
- Install crosswalks at the Butler Avenue's intersection with Lenape Drive, Tamenend Avenue, Iron Hill Road, and New Britain Road.
- Place wayfinding signs at various points within the corridor.
- Create a shared parking facility behind the commercial uses along the south side of Butler Avenue between Beulah Road and Shady Retreat Road.

A more detailed inventory of the proposed improvements is included below and is divided by the corridor's sub areas.

### Town Center Area

- Close Carousel Circle at Butler Avenue. This improvement would take place only if the Bristol Road extension was constructed. The Carousel Pointe development has two access points onto Butler Avenue in close proximity. Carousel Circle is the western point of access. Closing Carousel Circle would eliminate an access point onto Butler Avenue.
- Relocate driveway for TMP #25-001-009 (Del-Val International Trucks, Inc.). Align the driveway access point for the commercial property at 440 West Butler Avenue (TMP #25-1-9) with Pavilion way.

- Sidewalk construction. Sidewalk installation on north side of Butler Avenue from Bristol Road to Lenape Road. Also a new sidewalk on the Pine Run Elementary School site from Butler Avenue to the building's entrance.
- Median islands. Add raised median islands with plantings to areas with striping.
- Shoulder plantings. Fill the wide shoulder along the frontage of TMP #25-2-29 (Bucks Mont Party Rental) with plantings.
- Crosswalks. Add crosswalk at Lenape Drive.

#### Historic Village Area

- Crosswalks. Add a crosswalk at the eastern service road in front of TMP #25-2-134 and at the Tamenend Avenue intersection.
- Sidewalk construction. Construct missing sidewalk segment between the two service roads. Construct a sidewalk on the west side of Tamenend Avenue north of Butler Avenue. Sidewalks along the west side of South Tamenend Avenue from Butler Avenue south to New Britain Station.
- Wayfinding signs. Install wayfinding signs on Butler Avenue to the east and west of Tamenend Avenue and on South Tamenend Avenue.
- Reduction of open frontage at TMP #25-6-55. Install a planted edge along the Butler Avenue frontage of TMP #25-6-55. The site would then have access from Cedar Avenue.

#### University Village Area

- Shared parking. Construct shared parking for the commercial businesses on the south side of Butler Avenue from Beulah Road to Shady Retreat Road.
- Fill wide shoulder with planted edge. Fill the wide shoulder with a planting edge on the south side of Butler Avenue from Beulah Road to Shady Retreat Road.
- Reduction of open frontages. Reduce the open frontages on TMPs #25-6-57, 25-9-37, 25-9-38, 25-9-39, 25-9-19, and 25-9-77 by installing a planted edge.
- Wayfinding signs. Install wayfinding signs near the Delaware Valley University entrance.
- Gateway. Install a gateway feature at the intersection of Butler Avenue and New Britain Road.

These recommendations have been incorporated in the various chapters of this plan where applicable.

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## BUCKS COUNTY COMPREHENSIVE PLAN

The *Bucks County Comprehensive Plan* (2011) seeks to coordinate and assist the county's municipalities, agencies, and general public in the planning, development, and management of Bucks County's natural and built environment. The plan places a strong emphasis on sustainability and smart growth development strategies.

The Future Land Use Plan Map provides a framework for Smart Growth planning in Bucks County.

Smart Growth is a land use planning strategy which seeks to focus development on existing developed areas, preserve open space and natural resources, and link transportation and land use planning efforts. This map is meant to assist in the coordination and implementation of local and regional planning efforts.

New Britain Borough has been identified as a Secondary Town Centers on the county's Future Land Use Plan Map. Secondary Town Centers comprise the smaller boroughs within the County, for instance Dublin, Ivyland, Silverdale, and Yardley, and the larger villages like Croydon, Jamison, Richboro, and Southampton. Secondary Town Centers are similar to county's larger boroughs, which are designated as Town Centers, but do not have the full range of public services, infrastructure, or the mix or intensity of uses.

The plan advises that the Future Land Use Plan is not a site-specific land use plan but a generalized view of the types of land use that should be encouraged in the designated areas, and that local officials are encouraged to take into account local planning and zoning and current conditions when implementing the vision set forth in the comprehensive plan.

The plan suggests that given their key location and available infrastructure and services, Secondary Town Centers should serve as focal points for revitalization in accordance with Smart Growth principles. Development techniques, such as Transit-Oriented Development and Traditional Neighborhood Developments, will be especially important in the Secondary Town Center areas.

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## NEW BRITAIN BOROUGH ZONING ORDINANCE

The Borough's zoning ordinance controls land use within the corridor and the Borough. A description and analysis of each of the zoning districts is included below along with recommended changes to the ordinance. These recommendations are supported by both the current comprehensive plan and this corridor plan.

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### ZONING DISTRICTS

Within the study area all of the Borough's zoning districts, except the MHP Mobile Home Park and the P-OS Park and Open Space districts, are represented. Map 5 shows the Borough's zoning districts along with the corridor study area.

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### RESIDENTIAL DISTRICTS

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#### R-1 RESIDENTIAL DISTRICT

The R-1 Residential District is the borough's lowest density residential district. Within the study area this district is located on the north side of West Butler Avenue between Lenape Drive and Tamenend Avenue, on both sides of East Butler Avenue between Lamp Post Road and Heritage Lane, and on the north side of East Butler Avenue between Iron Hill Road and Evergreen Drive. The permitted

uses include single-family detached dwellings, public recreation facility, cemetery, forestry/timber harvesting, governmental office, temporary structure or use, and accessory uses associated with residential uses. Uses permitted by special exception include bed and breakfast, place of worship, school, and utilities. The minimum lot size for both residential and nonresidential uses is 20,000 square feet. Residential uses are permitted a maximum 25 percent impervious surface ratio and a maximum 20 percent building coverage. Nonresidential uses are permitted a maximum 60 percent impervious surface ratio and a maximum 20 percent building coverage.

The land use within the R-1 Residential District is predominantly single-family residential. Two adjacent parcels (TMPs #25-6-21 and 25-6-22) have been identified as rural residential. The New Britain Post Office has been categorized as an institutional land use.

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### R-2 RESIDENTIAL DISTRICT

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The R-2 Residential District is a higher density district intended to provide areas for multifamily, townhouse, and two-family dwellings. Within the study area the R-2 Residential District is located in two places. The first is the Carousel Pointe and Forest Park residential developments located at the far western end of the borough at the northeast corner of the intersection of Butler Avenue and Bristol Road. The other location is the Cedar Woods Townhouse development located at the northwest corner of Butler Avenue and Cedar Drive.

The permitted residential uses within the R-2 Residential District include single-family detached dwellings, two-family dwellings, multifamily, townhouse, and performance subdivision. Institutional uses that are appropriate in a residential setting including cemetery, government office, place of worship, and public recreation facility are also permitted.

The minimum lot size for single-family detached dwellings is 10,000 square feet, for two-family dwellings 5,000 square feet, and for multifamily and townhouse dwellings 2,000 square feet. Multifamily, townhouse, and performance subdivisions are required to provide 40 percent open space.

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### VH VILLAGE HISTORIC DISTRICT

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The VH Village Historic District is intended to protect the residential character of the area at the historic center of New Britain and to encourage the preservation of traditional structures. The zoning ordinance regulations are intended to preserve and promote the continuation of the residential uses and to allow home offices within the residences. The VH Village Historic District is centered on the intersection of Butler Avenue and Tamenend Avenue and on the south side of Butler Avenue and extends west to Lenape Drive and east to Lamp Post Road.

Single-family detached dwellings are the only residential use permitted within the district. Limited nonresidential uses are permitted including government office, place of worship, public recreational facility, and bed and breakfast. The minimum lot size for the district is 20,000 square feet.

## NONRESIDENTIAL DISTRICTS

A brief description of the nonresidential districts along with an analysis of the permitted uses to determine if these uses are compatible with the vision expressed for the corridor is found below. A table comparing the districts is also included for reference.

**Table 9. Nonresidential Zoning District Comparison**

	C-1 Commercial	C-2 Commercial	C-3 Commercial	HC Historic Commercial	SC Shopping Center	LI Light Industrial	C-U College/ University	E-I Educational Institutional District
Minimum lot area	40,000 sf	15,000 sf	20,000 sf	20,000 sf	3 acres	1 acre	10 acres	2 acres
Minimum lot width	100	100	100	100	150	150	100	150
Maximum impervious surface ratio	60 percent	60 percent	60 percent	35 percent	70 percent	50 percent	40 percent	30 percent
Maximum building coverage	25 percent	25 percent	20 percent	20 percent	40 percent	20 percent	25 percent	20 percent
Minimum yards (ft)								
Front	50	35	35	25	75	50	50	50
Side (each)	15	15	20	15	50	30	50	25
Rear	50	50	50	35	50	50	50	50

### C-1 COMMERCIAL DISTRICT

The C-1 Commercial District is intended to allow for commercial activities along Butler Avenue in areas where commercial establishments exist. The C-1 Commercial district is located on the south side of Butler Avenue from Bristol Road to Lenape Drive. Uses permitted by right within the district include banks, retail stores, offices, commercial recreational or sports facility, community centers, churches, and schools. Uses permitted by special exception include automobile sales and automobile service station or car wash.

This C-1 Commercial District permits the full range of commercial uses found within the Borough's zoning ordinance. These uses are predominately oriented toward "Main Street" style businesses. The possible exceptions include the auto-oriented uses (i.e., automobile sales and automobile service station or car wash). These uses are permitted by special exception; and therefore, would require review by the zoning hearing board. The zoning hearing board may then attach to the special exception permit conditions to meet the objectives of the zoning ordinance.

Automobile service station or car wash does not explicitly exclude the sale of retail items. Most newly developed gas stations also include a convenience store. The Borough should consider whether retail sales should be permitted with Automobile service station or car wash.

### C-2 COMMERCIAL DISTRICT

The C-2 Commercial District is intended to allow for small-scale neighborhood commercial, service, and office uses along Butler Avenue. Within the corridor the C-2 Commercial District is found at 341, 401, and 433 West Butler Avenue (TMP#s 25-1-4, 25-1-3, and 25-1-3-1), on the south side of East

Butler Avenue between Beulah Road and Shady Retreat Road, at the Smith Marine/Alley-Gator Shop (573 East Butler Avenue, TMP#s 25-9-19 and -20), Rissi's Automotive Service, Inc. (749 East Butler Avenue, TMP #25-9-77), and the American Heritage Federal Credit Union (765 East Butler Avenue, TMP #25-9-79).

The C-2 Commercial District permits by right many of the same uses as the C-1 Commercial District such as banks, retail stores, offices, eating places, commercial recreational or sports facility, community centers, churches, and schools. An additional use only allowed in the C-2 Commercial district is the motel or hotel use which is permitted by right. The uses permitted within the C-2 Commercial District are oriented toward "Main Street" style businesses.

### C-3 OFFICE COMMERCIAL DISTRICT

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The C-3 Office Commercial District is intended to allow for professional offices on properties along Butler Avenue. The C-3 Office Commercial District is located, from west to east, at the Gilmore office property (65 East Butler Avenue, TMP #25-3-22) on the north side of Butler Avenue between Keeley Avenue and Lamp Post Road; at the law offices of Sweet, Stevens, Katz, and Williams (331 East Butler Avenue, TMP #25-6-47) on the north side of Butler Avenue across from Heritage Lane; at the office complex at 350 and 352 East Butler Avenue (TMP #25-6-13); on the north side of East Butler Avenue from Cooks Run to Shady Retreat Road, and at the northwest and southwest corner of the Iron Hill Road intersection.

The C-3 Commercial District is the Borough's office district. The permitted uses and the land use within the district reflect that description. Office uses permitted by right include government office, medical office, business office, and veterinary office. Places of worship, public recreational facility, cemetery, and single-family detached dwellings are also permitted. The uses permitted within the C-3 Commercial District are oriented toward "Main Street" style businesses.

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### HC HISTORIC COMMERCIAL DISTRICT

The HC Historic Commercial district is intended to promote and preserve the historic and limited fabrication/commercial character of the area around the historic Godshalk's Mill. Within the corridor the HC Historic Commercial District is found on either side of the C-3 Commercial District that encompasses the office complex at 350 and 352 East Butler Avenue (TMP #25-6-13). Behind the office complex is a Healey's Auto Service (354 East Butler Avenue, TMP #25-6-13-1) which shares a driveway with the office complex.

The permitted uses within the HC Historic Commercial District include craft village, offices, public recreational facility, and repair shop. A craft village is a facility or group of structures and buildings where craftsmen or artisans manufacture items for sale for personal or household use, such as cabinetmakers, furniture makers, and makers of artistic or craft items. A craft village is only permitted on a lot with an historic structure, as identified by the Borough, present. While the existing auto repair

is a nonconforming use within the HC District and does not fit into the vision for the Historic Village area of the corridor, its setback from the road mitigates its impact. The uses permitted within the HC Historic Commercial District are generally compatible with “Main Street” style businesses.

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#### *SC SHOPPING CENTER DISTRICT*

The SC Shopping Center District is intended to provide for retail and service uses and office uses in a coordinated shopping center accessible from arterial roads. The SC Shopping Center District is only found at the Town Center of New Britain shopping center. Uses permitted in the district include banks, beer/soda distributor, convenience store, day care center, restaurant, offices, shopping center, and retail stores. The shopping center use permits retail trade or service business, restaurant, repair shop, large retail store, financial establishment, and commercial recreation facility. Drive through restaurants are permitted as a conditional use.

These uses are compatible with the shopping center as it exists now. If further development of the shopping center occurs, especially development near the road, it should be developed with regard to fitting in with the context of a “Main Street.”

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#### *LI LIGHT INDUSTRIAL DISTRICT*

The LI Light Industrial District is intended to permit light industrial development located along the SEPTA rail line, ensure that industrial development has sufficient controls to preserve the overall residential character of the Borough, protect environmental features, and to ensure that development along Butler Avenue is consistent with the Borough’s corridor planning and streetscape standards. Within the corridor, the Light Industrial District is located at the Knoell site (TMP #25-9-2), the Delaware Valley Concrete site (TMP #25-8-3-1), and the rear portion of the Bucks-Mont Party Rental store (TMP #25-9-29).

The Light Industrial District permits by right motel or hotel, offices, public recreation facility, and research. The LI Light Industrial District regulations prohibit uses that do not fit into the “Main Street” streetscape that has been envisioned for the corridor. These uses are not permitted on lots that front or take access from Butler Avenue: Adult Commercial; Automobile body repair or paint shop; Fuel storage and distribution; Light manufacturing; Telecommunications facility; Truck sales; Warehousing and distribution; and Wholesale trade. However, these uses are permitted by special exception on lots not fronting on Butler Avenue.

The Delaware Valley Concrete site (TMP #25-8-3-1) does not front on or take access from Butler Avenue. Therefore, the above mentioned uses may be permitted by special exception. These uses should be prohibited on this site to help ensure that redevelopment of this site would be in harmony with the vision for Butler Avenue.

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### EDUCATIONAL INSTITUTIONAL DISTRICT

The E-I Educational Institutional District is intended to maintain the character of areas currently used by educational institutions and hospitals. The Pine Run Elementary School and Foundations Behavioral Health facility are the only properties within the E-I Educational Institutional District. The district allows for hospitals and schools along with other institutional uses and office uses. Pine Run Elementary School and Foundations Behavioral Health are both well-established land uses that will most likely continue in their present form.

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### COLLEGE UNIVERSITY DISTRICT

The purpose of the C-U College University District is to allow for Delaware Valley University and its associated uses. The district is found only on the campus of the university. The district permits, in addition to a college or university, day-care center, dormitory, office uses, and other institutional uses. Bed and breakfast, commercial recreational sports facility, and eating place as accessory to a college are permitted as special exceptions. These uses are would generally compliment a “Main Street” character.

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### OVERLAY DISTRICTS

An overlay district is a zoning district which is applied over one or more previously established zoning districts, establishing additional or stricter standards and criteria for covered properties in addition to those of the underlying zoning district. Overlay districts can be used to protect special features such as historic buildings or environmentally sensitive areas. Overlay zones can also be used to promote specific development projects, such as mixed-used developments, housing along transit corridors, or affordable housing.

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### MIXED USE OVERLAY

The MUO-Mixed Use Overlay District was recently adopted by Borough officials for the University Village area. Map 6 shows the MUO District boundaries. The MUO District is intended to encourage development and revitalization along Butler Avenue that would contribute to the “Main Street” character envisioned in the *New Britain Borough Comprehensive Plan* (2009) and the *Butler Avenue Main Street Study* (2016). The provisions of the overlay district allow for a Mixed-Use Development, with a combination of two or more uses, on lots greater than 1.0 acres. A minimum of 1,000 square feet of nonresidential use for every 10 residential units is required. Single use development is permitted on lots less than 1.0 acre. Nonresidential uses permitted include banks, bed and breakfast, commercial recreational and sports facility, convenience store, day care center, cultural facility, office, motel or hotel, nonresidential accessory structure, personal service business, public recreation facility, research,

retail trade and services, eating place, and state liquor store. Residential uses that are permitted include single-family detached dwelling, townhouse dwelling, and multifamily dwelling.

Requirements to encourage the desired “Main Street” character include parking along Butler Avenue to the rear of the principal buildings, streetscape improvements, limited vehicular access points, and public space (e.g., plazas, seating areas, outdoor eating areas, and wide sidewalks) visible from Butler Avenue. An architectural design review by the Borough is required for any new structure or improvement to any land within the District.

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#### *HISTORIC PRESERVATION OVERLAY DISTRICT*

The Borough has identified buildings in the Borough that have historical, architectural, and or cultural importance and has established the corresponding criteria for the identification of these buildings. To prevent the demolition of such buildings, procedures and standards are required which are intended to assess the impact that the demolition of such structures would have upon the historic, cultural, and or architectural values within the Borough. Map 7 shows the buildings included in the Historic Preservation Overlay District.

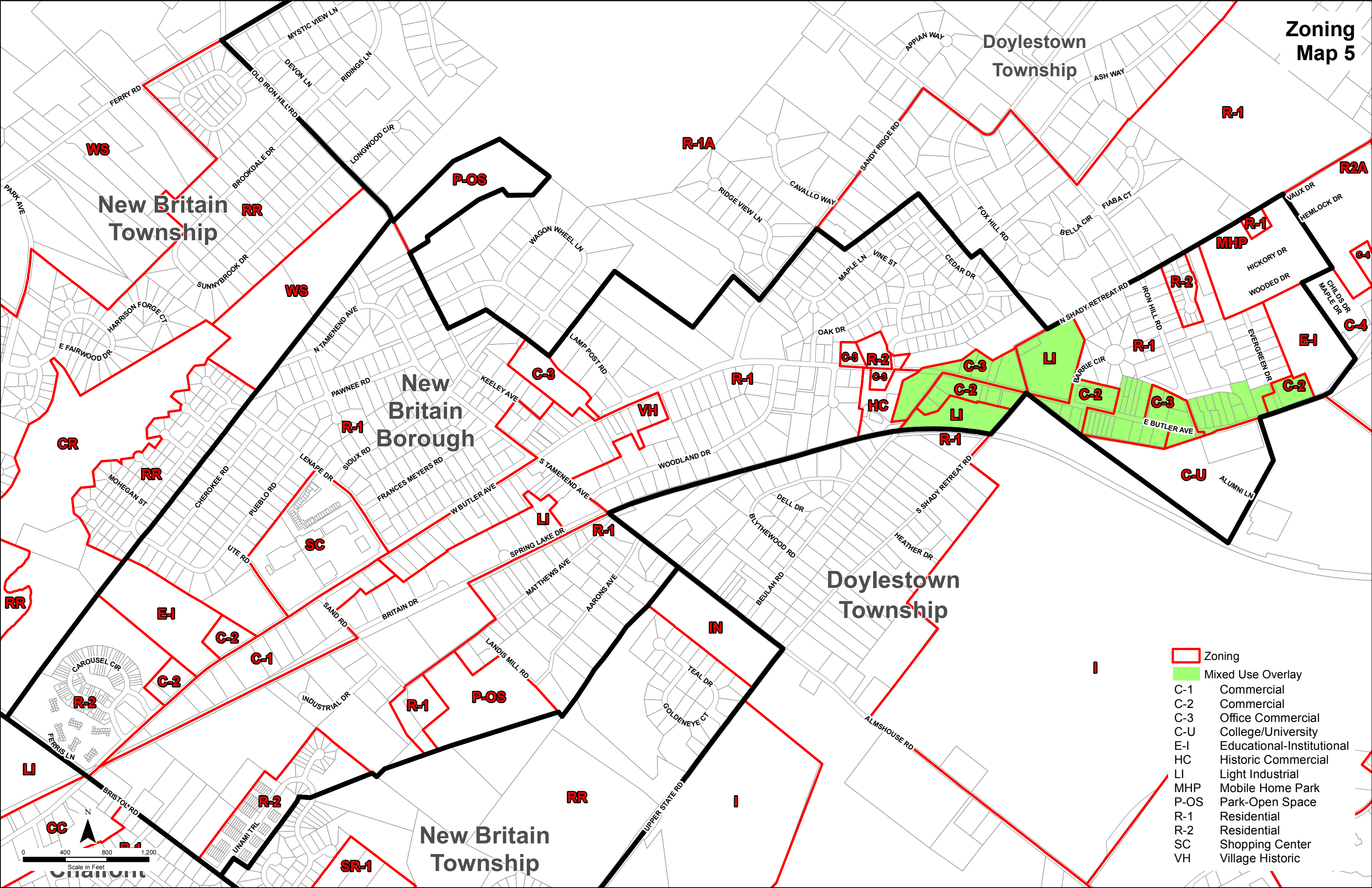
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#### *STUDENT HOME OVERLAY DISTRICT*

The Student Home Overlay District provides for off-campus student housing within the R-1 Residential, the R-2 Residential, and the VH Village Historic districts by special exception. A student home is defined a residential living arrangement for no more than three students.

District regulations permit student homes in single-family detached dwellings, townhouse dwellings, mobile homes, and two-family dwellings with a minimum separation of 500 feet between each student home.

Zoning Map 5



## ECONOMIC CONDITIONS

Like any community, New Britain's economic well-being is tied to that of the larger national, state, and regional trends. These trends in national economic conditions are complex and not readily remedied at the local level. Given these circumstances, it is important for a community to be aware of the economic conditions within its reach and to be proactive in its approach to economic development.

An analysis and examination of various factors including economic conditions and characteristics of the local retail market helps identify potential economic development opportunities. The following analysis is used as the basis for an economic development vision for corridor.

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### RETAIL MARKET ANALYSIS

Maintaining and enhancing commercial development is important to the vitality of New Britain Borough Commercial Area. Evaluating the local retail market is useful to identify potential economic opportunities in the retail sector. The assessment of local market conditions for New Britain Borough Commercial Area begins by gathering relevant population, household, and other demographic data. By drawing on available census and analysis tools, the boundaries of the New Britain Borough Commercial Area market can be based on drive times and demographic trends summarized using the 2000, 2010, 2015, and 2020 time periods. Such tools are used to determine consumer expenditures within the market, understand which market segment these consumers occupy, and show areas of potential economic opportunity. It should be noted that demographic data was discussed in a previous chapter of the study. The data presented here goes into a more detailed analysis as it relates to economic development issues.

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### MARKET BOUNDARIES AND DEMOGRAPHICS

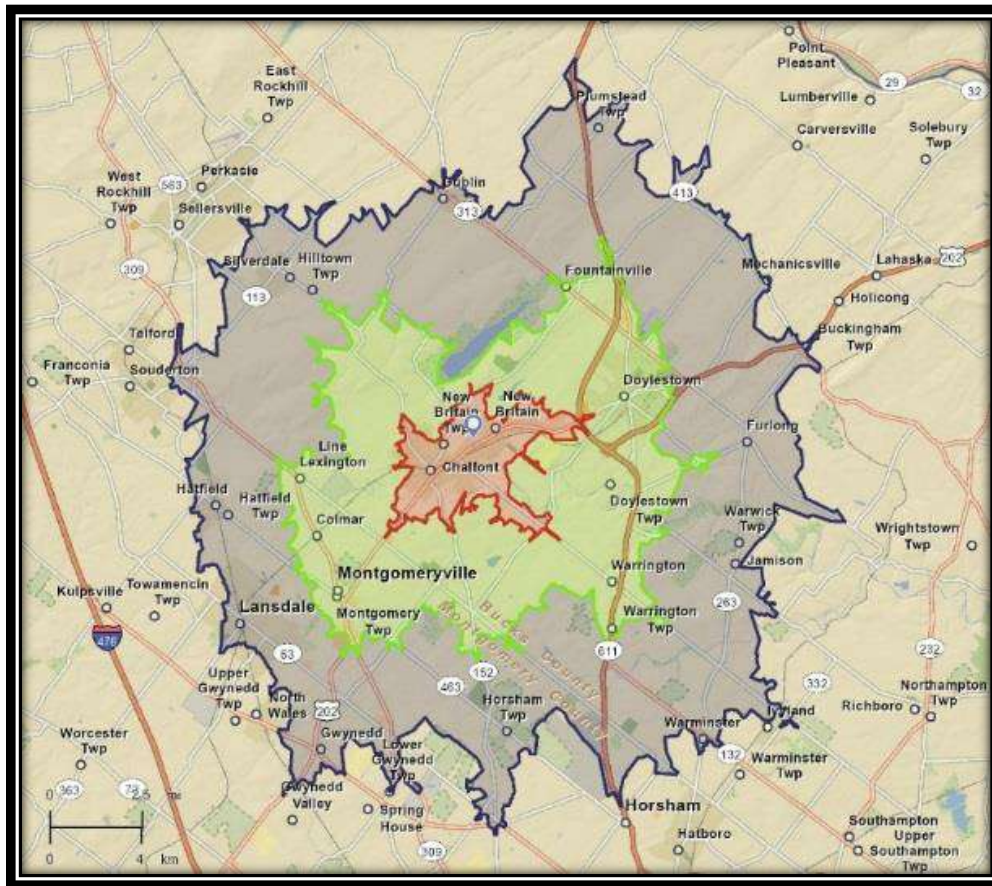
Using the ESRI Business Analyst<sup>9</sup> tool the boundaries of the New Britain Borough Commercial Area Market are divided into 0-5, 0-10, and 0-15-minute drive times<sup>10</sup> from the center of the borough, which for the purposes of this study was determined to be at the intersection of Butler Avenue and Tamenend Avenue). Basing the drive time analysis from this point was seen as a compromise to include both the western and eastern commercial areas. (See Figure 7). The following narrative and tables provide demographic and analytic data within the boundaries of these drive times (detailed ESRI market analysis tables follow).

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<sup>9</sup> ESRI Community Analyst is a web-based mapping and analytic tool that allows for analysis of demographic, economic, education, and business data.

<sup>10</sup> Drive time intervals are areas defined by distance that can be driven away from a specific location within a specified time (in minutes) assuming posted speed limits for the road network. Barriers such as mountains, rivers, bridges, or highways under normal traffic conditions are taken into account when establishing the boundaries.

Figure 7. New Britain Borough. Commercial Area – 5, 10, 15 Minute Drive Time Market Areas



Source: ESRI Business Analyst, 2015.

Table 10. Population Summary

	0 - 5 minutes	0 - 10 minutes	0 - 15 minutes	New Britain Boro.	Bucks County
2000 Total Population	11,096	69,103	147,331	3,125	597,636
2010 Total Population	11,152	74,990	169,321	3,152	625,249
2015 Total Population	11,101	76,181	172,755	3,155	631,027*
2020 Total Population	11,095	77,447	175,574	3,160	646,435*
2010-2020 Annual Rate	-0.005%	0.33%	0.35%	0.003%	0.34%

Source: ESRI Business Analyst, 2015 \*DVRPC 2040 Population Forecasts.

The market population in the three drive-time categories, as shown in Table 10, looks to be significant. However, this must be tempered by the fact that the demand for many of the types of goods and services offered the study area will be from customers within the 0-10-minute drive-time. Overall the population within the target markets has seen a slight increase since 2000. The 0-5 minute market will essentially stay the same from 2000 to 2020. Therefore, it appears that the target market population will continue to increase except for a slight decrease in the 5-minute market.

**Table 11. Household Summary**

	0 - 5 minutes	0 - 10 minutes	0 - 15 minutes
2010 Households	4,100	27,312	61,197
2010 Average Household Size	2.44	2.63	2.70
2015 Households	4,104	27,987	62,748
2015 Average Household Size	2.43	2.61	2.69
2020 Households	4,105	28,540	63,956
2020 Average Household Size	2.42	2.60	2.68
2015-2020 Annual Rate	-0.0%	0.39%	0.38%

*Source: ESRI Business Analyst, 2015*

As with population, an increase in the number of households is occurring, except in the 5-minute market where the number has been held constant. Household sizes, however are slightly decreasing. In 2010, the average household size was 2.44 persons in the 0-5 minute drive-time market. By 2020 it is expected to drop to 2.42 persons. Household size has been decreasing all over Bucks County. This is due largely to an aging population, a decline in the birth rate, and more singles living alone.

**Table 12. 2010 Households by Type**

	0 - 5 minutes	0 - 10 minutes	0 - 15 minutes
Total	4,100	27,312	61,197
Family Households	69.7%	72.0%	74.1%
Married-couple Family	59.3%	61.8%	63.6%
With Related Children	25.3%	30.2%	31.1%
Other Family (No Spouse)	10.4%	10.2%	10.5%
With Related Children	6.5%	6.8%	5.9%
Nonfamily Households	4.9%	4.8%	4.6%
All Households with Children	31.4%	36.1%	37.0%

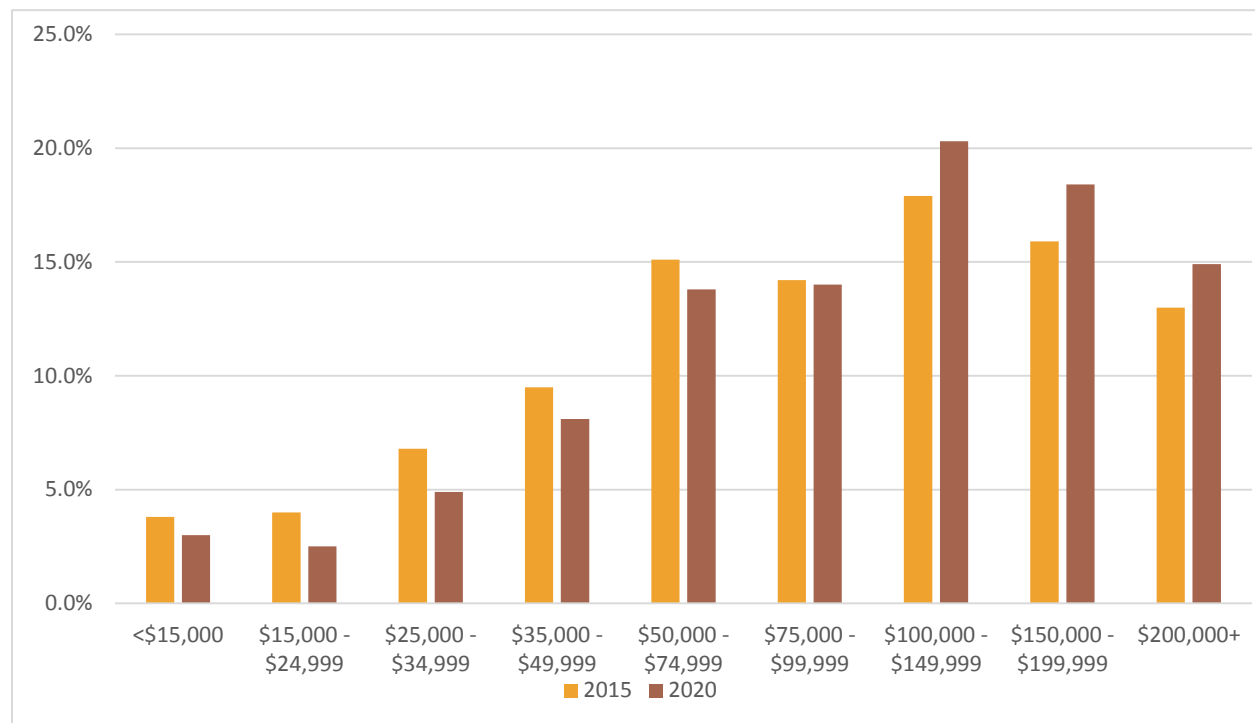
Household composition fluctuates from the 5-minute market to the 15-minute market. Almost 70 percent of households in the 0-5-minute drive-time market are family households, compared to 72.0 percent in the 0-10-minute drive-time market and 74.1 percent in the 0-15 minute drive-time market. About 25.3 percent of households in the 0-5 minute market have related children living in the household. This increases to 30.2 percent in the 0-10 minute drive-time market then up to 31.1 percent in the 0-15-minute drive-time market. The percentage of Nonfamily Households, which would include householders living alone, is relatively consistent as the market area expands from the center of the commercial area. Within the 0-5 minute market area 4.9 percent of households are Nonfamily Households, which is partially attributable to the University population. This measure decrease to 4.8 percent the 0-10-minute market and then drops again to 4.6 percent in 0-15-minute drive-time market.

**Table 13. Median Household Income**

	0 - 5 minutes	0 - 10 minutes	0 - 15 minutes
2015	\$92,871	\$98,211	\$100,256
2020	\$106,114	\$110,754	\$111,054

A clear difference in markets emerges when examining household income. Residents within a 5-minute drive time earn nearly \$6,000 less than residents in the 0-10-minute market and nearly \$8,000 less than residents in the 0-15-minute markets. By 2020, this difference is expected to narrow across all categories with the 0-10 and 0-15 minute markets less than \$1,000 apart.

**Figure 8. Median Household Income by Income Bracket, 5-minute Drive**



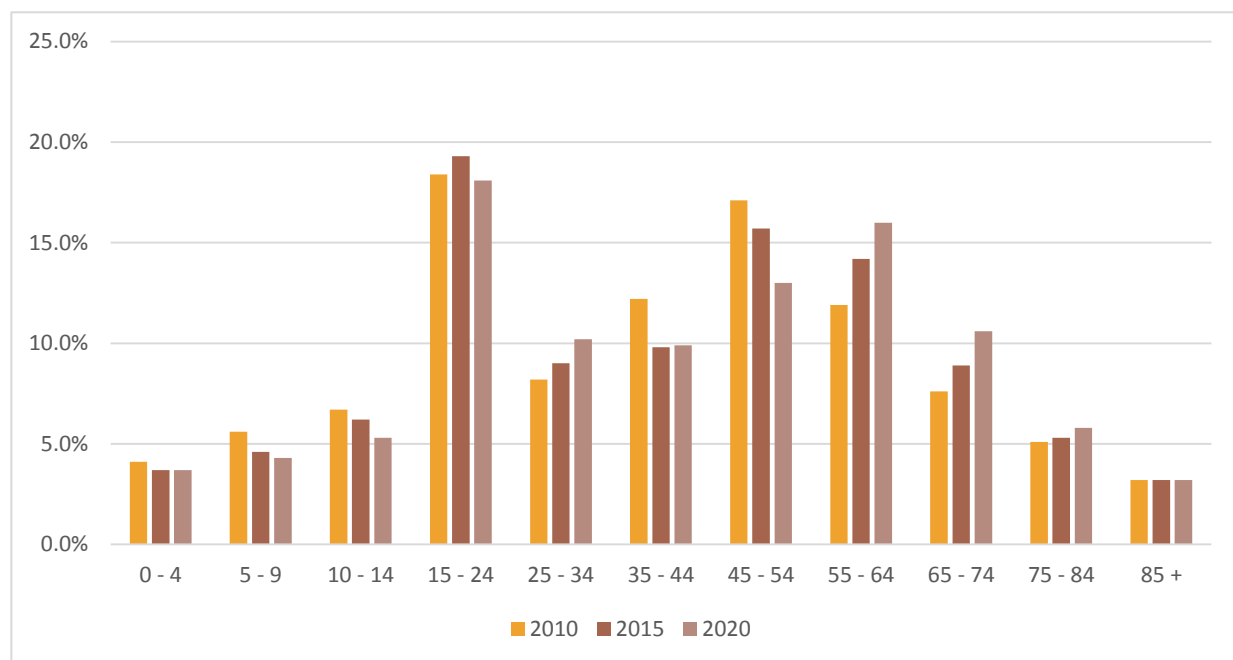
The rise in median household income looks promising, but is even more promising when household income is divided into income brackets as shown in Figure 8 above. Household incomes over \$100,000 within a 5-minute drive will represent over 50 percent of the households by 2020.

**Table 14. Median Age**

	0 - 5 minutes	0 - 10 minutes	0 - 15 minutes
2010	41.3	41.7	41.5
2015	42.7	43.2	42.8
2020	43.5	44.8	44.2

Much like the rest of Bucks County, residents within all three drive-time markets are getting older. The highest median age, as of 2015, is associated with residents living within 10 minutes of the business district (43.2 years), followed by the median ages of residents living within 15 minutes (42.8 years) and residents living within 5 minutes (42.7 years).

**Figure 9. Population Percentage by Age, 5-minute Drive**



While the population may becoming wealthier, it is not becoming any younger. In 2010, the market consisted of a good portion of middle-aged residents and has since skewed toward retiring baby boomers and will further move in this direction as the population continues to age. Baby boomers will likely handle retirement differently than the previous generation of retirees. Demographers suggest that boomers will spend more time and money on personal needs and desires, be less civically engaged, more physically active, and more interested in living in a diverse and urban environment (similar to that of young adults). It must be noted that the target markets will continue to have a significant number of high school-age children and young adults as reflected in the 15 to 24 age group. This group also represents the college-aged students attending Delaware Valley University and living within the market area.

**Table 15. 2013 Population by Race/Ethnicity**

	0 - 5 minutes	0 - 10 minutes	0 - 15 minutes
Total	11,152	76,181	194,756
White Alone	94.3%	89.9%	86.7%
Black Alone	2.0%	2.3%	3.0%
American Indian Alone	0.2%	0.1%	0.2%
Asian or Pacific Islander Alone	2.4%	5.3%	7.4%
Some Other Race Alone	0.7%	0.8%	1.1%
Two or More Races	1.4%	1.5%	1.6%
Hispanic Origin	2.7%	3.2%	3.7%

Population diversity increases as one drives from the center of the commercial area. Whites account for 94.3 percent of the population within the 5-minute drive boundary. The Black population rises from 2.0 percent to 3 percent moving from the 5-minute to the 15-minute market area, while the Asian population ranges from 2.4 percent to 7.4 percent. The Hispanic population within the 5-minute drive boundary is 2.7 percent, lower than that the County percentage, but it increases to 3.2 percent in the 15-minute drive boundary.

**Table 16. 2015 Population 25+ by Educational Attainment**

	0 - 5 minutes	0 - 10 minutes	0 - 15 minutes	Bucks County*
Total	7,342	52,468	118,741	429,091
Less Than 9th Grade	1.2%	1.2%	1.4%	2.4%
9th to 12th Grade, No Diploma	3.6%	2.8%	2.8%	5.7%
High School Graduate	22.5%	20.8%	21.1%	31.6%
Some College, No Degree	15.8%	14.8%	15.3%	18.4%
Associate Degree	7.4%	7.0%	6.9%	7.3%
Bachelor's Degree	28.6%	31.3%	30.6%	21.2%
Graduate/Professional Degree	19.5%	20.4%	20.2%	13.4%

*\*Bucks County data for 2010*

The differences in household income appear to be generally reflective of the educational attainment of the three market areas. About 48.1 percent of residents in the 0-5-minute market have obtained a bachelor's or graduate/professional degree, compared to 51.7 percent of residents in the 0-10-minute market and 50.8 percent of residents in the 0-15-minute market. These percentages are higher than Bucks County as a whole, of which 34.6 percent of residents in 2010 earned bachelors or graduate/professional degrees.

**Table 17. 2015 Employed Population 16+ by Occupation**

	0 - 5 minutes	0 - 10 minutes	0 - 15 minutes
Total	5,874	40,423	92,008
White Collar	71.0%	76.1%	75.7%
Management/Business/Financial	16.6%	21.2%	21.2%
Professional	30.0%	29.5%	28.0%
Sales	11.8%	12.5%	12.9%
Administrative Support	12.6%	13.0%	13.6%
Services	13.9%	11.5%	11.4%
Blue Collar	15.1%	12.4%	12.9%
Farming/Forestry/Fishing	0.7%	0.3%	0.2%
Construction/Extraction	5.2%	3.4%	4.0%
Installation/Maintenance/Repair	2.6%	2.0%	2.0%
Production	2.1%	2.4%	3.1%
Transportation/Material Moving	4.5%	4.2%	3.6%

The percentage of white collar, services, and blue collar occupations for the three market areas is remarkably consistent. White collar occupations comprise the vast majority of occupations and range between 71 percent in the 5-minute market area to 76.1 percent in the 10-minute market. Services range between from 11.4 percent in the 15-minute market area to 13.9 percent in the 5-minute market. Blue collar occupations range between 15.1 percent in the 5-minute market area to 12.4 percent in the 10-minute market.

**Table 18. Housing Unit Summary**

	0 - 5 minutes	0 - 10 minutes	0 - 15 minutes
2010 Housing Units	4,358	28,442	63,719
Owner Occupied Housing Units	74.3%	79.1%	80.2%
Renter Occupied Housing Units	19.8%	16.9%	16.1%
Vacant Housing Units	5.9%	4.0%	4.0%
2015 Housing Units	4,503	29,130	65,177
Owner Occupied Housing Units	70.7%	77.5%	78.6%
Renter Occupied Housing Units	20.5%	18.6%	17.6%
Vacant Housing Units	8.9%	3.9%	3.7%
2020 Housing Units	4,545	29,685	66,251
Owner Occupied Housing Units	70.2%	77.4%	78.8%
Renter Occupied Housing Units	20.1%	18.8%	17.8%
Vacant Housing Units	9.7%	3.9%	3.5%

Housing tenure differs in the three markets. About 84 percent of units in 2010 were owner-occupied in the 0-5-minute drive time area. This percentage only reached 77.2 percent and 64.2 percent in the 0-10-minute and 0-15-minute areas, respectively. Vacancy rates increased as the drive times fluctuated from the center of the market area. Each market area experienced a decline in the percentage of owner-occupied units from 2000 to 2010 and this is expected to further decline to the year 2020.

**Table 19. Median Home Value**

	0 - 5 minutes	0 - 10 minutes	0 - 15 minutes
2015	\$368,758	\$386,068	\$382,897
2020	\$409,560	\$433,455	\$427,884

Home values are significant in and near the Borough, but they are greater farther from the center, as most likely influenced by home values in the Doylestown Borough area. By 2020, median home values are expected to be nearly \$410,000 within a 5-minute drive of the business district. In the 0-10-minute market, they rise to about \$433,000 and fall slightly in the 15-minute market to a nearly \$428,000.

#### *RETAIL GOODS AND EXPENDITURES*

The following data represents average household expenditures for retail goods and services within the specified market areas. A Spending Potential Index (SPI) was developed from the ESRI Business Analyst and represents the amount spent for a product or service relative to a national average of 100. Consumer spending data are derived from 2011 and 2012 Consumer Expenditure Surveys from the Bureau of Labor Statistics.

**Table 20. Spending Potential Index**

	5-Minute Drive Time			10-Minute Drive Time			15-Minute Drive Time		
	SPI	Average Amount Spent	Total	SPI	Average Amount Spent	Total	SPI	Average Amount Spent	Total
<b>Apparel and Services</b>	157	\$3,634.12	\$14,914,427	163	\$3,772.19	\$105,572,393	163	\$3,782.12	\$237,320,687
Men's	159	\$691.15	\$2,836,484	165	\$717.20	\$20,072,300	166	\$719.37	\$45,138,770
Women's	160	\$1,287.21	\$5,282,701	165	\$1,331.35	\$37,260,379	165	\$1,328.76	\$83,377,029
Children's	149	\$557.63	\$2,288,505	156	\$583.57	\$16,332,290	158	\$591.50	\$37,115,382
Footwear	150	\$687.12	\$2,819,921	156	\$712.91	\$19,952,288	157	\$716.79	\$44,977,225
Watches & Jewelry	171	\$247.75	\$1,016,748	177	\$256.80	\$7,187,152	176	\$255.49	\$16,031,376
Apparel Products and Services (1)	167	\$163.27	\$670,068	174	\$170.36	\$4,767,985	174	\$170.22	\$10,680,904
<b>Computer</b>									
Computers and Hardware for Home Use	161	\$348.91	\$1,431,947	167	\$361.56	\$10,119,082	167	\$361.55	\$22,686,601
Portable Memory	158	\$8.51	\$34,925	162	\$8.74	\$244,664	162	\$8.73	\$547,732
Computer Software	161	\$32.65	\$133,978	167	\$33.77	\$945,202	167	\$33.76	\$2,118,670
Computer Accessories	167	\$32.02	\$131,397	172	\$32.80	\$917,906	171	\$32.65	\$2,048,826
<b>Entertainment &amp; Recreation</b>	162	\$5,379.45	\$22,077,252	168	\$5,549.93	\$155,325,832	167	\$5,539.86	\$347,615,045
Fees and Admissions	181	\$1,173.85	\$4,817,487	190	\$1,229.98	\$34,423,577	190	\$1,229.87	\$77,171,874
Membership Fees for Clubs (2)	186	\$318.86	\$1,308,621	195	\$334.38	\$9,358,201	194	\$332.54	\$20,866,077
Fees for Participant Sports, excl. Trips	180	\$217.07	\$890,844	188	\$226.85	\$6,348,760	187	\$225.74	\$14,164,903
Admission to Movie/Theatre/Opera/Ballet	171	\$281.74	\$1,156,256	177	\$291.88	\$8,168,884	177	\$292.12	\$18,329,788
Admission to Sporting Events, excl. Trips	183	\$121.54	\$498,808	192	\$128.03	\$3,583,211	191	\$127.34	\$7,990,459
Fees for Recreational Lessons	190	\$233.89	\$959,880	202	\$248.12	\$6,944,152	205	\$251.41	\$15,775,374
Dating Services	125	\$0.75	\$3,079	122	\$0.73	\$20,369	120	\$0.72	\$45,274
TV/Video/Audio	153	\$2,005.54	\$8,230,742	157	\$2,051.95	\$57,427,988	157	\$2,049.62	\$128,609,449
Cable and Satellite Television Services	151	\$1,349.48	\$5,538,270	153	\$1,373.68	\$38,445,249	153	\$1,370.91	\$86,022,065
Televisions	157	\$232.07	\$952,413	161	\$238.10	\$6,663,695	161	\$237.67	\$14,913,594
Satellite Dishes	149	\$2.34	\$9,604	152	\$2.39	\$66,992	150	\$2.35	\$147,764
VCRs, Video Cameras, and DVD Players	154	\$16.87	\$69,234	158	\$17.35	\$485,558	158	\$17.40	\$1,091,904
Miscellaneous Video Equipment	169	\$18.21	\$74,729	182	\$19.57	\$547,757	181	\$19.45	\$1,220,338
Video Cassettes and DVDs	151	\$48.53	\$199,149	155	\$49.74	\$1,392,091	154	\$49.51	\$3,106,949
Video Game Hardware/Accessories	139	\$32.14	\$131,901	144	\$33.28	\$931,471	145	\$33.43	\$2,097,957
Video Game Software	147	\$40.47	\$166,082	151	\$41.56	\$1,163,162	152	\$41.85	\$2,626,106
Streaming/Downloaded Video	165	\$9.51	\$39,028	172	\$9.90	\$277,006	172	\$9.90	\$621,503
Rental of Video Cassettes and DVDs	149	\$35.01	\$143,662	153	\$36.00	\$1,007,488	153	\$35.93	\$2,254,447
Installation of Televisions	192	\$2.15	\$8,804	209	\$2.34	\$65,601	210	\$2.35	\$147,695
Audio (3)	171	\$210.51	\$863,936	178	\$219.59	\$6,145,537	179	\$220.45	\$13,832,600
Rental and Repair of TV/Radio/Sound Equipment	154	\$8.27	\$33,930	157	\$8.45	\$236,381	156	\$8.39	\$526,527
Pets	157	\$891.97	\$3,660,657	161	\$914.83	\$25,603,379	160	\$909.20	\$57,050,480
Toys and Games (4)	155	\$190.23	\$780,687	159	\$194.68	\$5,448,453	159	\$195.67	\$12,278,129
Recreational Vehicles and Fees (5)	173	\$376.76	\$1,546,219	180	\$392.17	\$10,975,657	180	\$391.14	\$24,543,182
Sports/Recreation/Exercise Equipment (6)	159	\$301.13	\$1,235,830	166	\$314.58	\$8,804,128	166	\$314.38	\$19,726,906
Photo Equipment and Supplies (7)	169	\$137.02	\$562,328	175	\$142.55	\$3,989,468	175	\$142.46	\$8,938,796
Reading (8)	170	\$259.31	\$1,064,215	173	\$263.77	\$7,382,184	172	\$261.98	\$16,438,479
Catered Affairs (9)	183	\$43.64	\$179,087	191	\$45.41	\$1,271,000	191	\$45.54	\$2,857,751

**Table 20. Spending Potential Index (continued)**

	5-Minute Drive Time			10-Minute Drive Time			15-Minute Drive Time		
	SPI	Average Amount Spent	Total	SPI	Average Amount Spent	Total	SPI	Average Amount Spent	Total
<b>Food</b>	155	\$13,154.64	\$53,986,661	159	\$13,568.37	\$379,738,058	160	\$13,587.15	\$852,566,706
Food at Home	152	\$7,954.11	\$32,643,683	157	\$8,180.37	\$228,943,917	157	\$8,186.79	\$513,704,943
Bakery and Cereal Products	153	\$1,114.46	\$4,573,757	157	\$1,145.34	\$32,054,504	157	\$1,147.13	\$71,979,883
Meats, Poultry, Fish, and Eggs	151	\$1,732.66	\$7,110,848	155	\$1,784.32	\$49,937,868	156	\$1,787.44	\$112,158,431
Dairy Products	153	\$863.61	\$3,544,268	158	\$888.66	\$24,870,797	158	\$890.13	\$55,853,807
Fruits and Vegetables	156	\$1,539.60	\$6,318,502	161	\$1,584.48	\$44,344,902	161	\$1,585.93	\$99,514,015
Snacks and Other Food at Home (10)	151	\$2,703.78	\$11,096,308	155	\$2,777.57	\$77,735,846	155	\$2,776.17	\$174,198,807
Food Away from Home	158	\$5,200.53	\$21,342,978	164	\$5,388.01	\$150,794,141	164	\$5,400.36	\$338,861,763
Alcoholic Beverages	164	\$912.21	\$3,743,717	169	\$936.71	\$26,215,795	169	\$937.48	\$58,825,116
Nonalcoholic Beverages	149	\$742.70	\$3,048,052	153	\$761.93	\$21,324,045	153	\$762.63	\$47,853,423
<b>Financial</b>									
Investments	177	\$4,875.45	\$20,008,846	177	\$4,882.02	\$136,633,119	168	\$4,631.16	\$290,596,302
Vehicle Loans	155	\$6,550.34	\$26,882,578	160	\$6,761.96	\$189,246,883	160	\$6,776.35	\$425,202,395
<b>Health</b>									
Nonprescription Drugs	153	\$198.35	\$814,045	157	\$203.25	\$5,688,236	156	\$201.74	\$12,658,784
Prescription Drugs	156	\$778.45	\$3,194,757	159	\$789.87	\$22,106,087	157	\$781.25	\$49,021,924
Eyeglasses and Contact Lenses	164	\$147.73	\$606,270	169	\$151.89	\$4,251,054	168	\$151.45	\$9,503,346
<b>Home</b>									
Mortgage Payment and Basics (11)	181	\$16,953.76	\$69,578,248	188	\$17,612.62	\$492,924,310	188	\$17,632.05	\$1,106,375,773
Maintenance and Remodeling Services	184	\$3,102.07	\$12,730,913	190	\$3,210.64	\$89,856,303	190	\$3,197.72	\$200,650,798
Maintenance and Remodeling Materials (12)	162	\$486.24	\$1,995,537	166	\$497.02	\$13,910,173	165	\$495.92	\$31,118,227
Utilities, Fuel, and Public Services	154	\$7,782.18	\$31,938,060	157	\$7,953.71	\$222,600,539	157	\$7,946.27	\$498,612,347
<b>Household Furnishings and Equipment</b>									
Household Textiles (13)	163	\$160.70	\$659,493	167	\$164.72	\$4,609,972	167	\$164.66	\$10,331,967
Furniture	161	\$832.09	\$3,414,892	168	\$868.28	\$24,300,655	168	\$867.45	\$54,430,861
Rugs	186	\$45.87	\$188,251	192	\$47.33	\$1,324,728	192	\$47.34	\$2,970,439
Major Appliances (14)	164	\$441.89	\$1,813,518	169	\$455.08	\$12,736,422	169	\$453.68	\$28,467,757
Housewares (15)	162	\$117.08	\$480,509	167	\$120.25	\$3,365,317	166	\$119.74	\$7,513,722
Small Appliances	157	\$71.80	\$294,664	159	\$72.94	\$2,041,300	159	\$72.80	\$4,567,898
Luggage	177	\$16.24	\$66,668	185	\$16.96	\$474,624	184	\$16.92	\$1,061,465
Telephones and Accessories	147	\$73.47	\$301,535	153	\$76.44	\$2,139,451	153	\$76.48	\$4,798,751
<b>Household Operations</b>									
Child Care	164	\$735.67	\$3,019,199	173	\$775.44	\$21,702,147	177	\$791.24	\$49,648,711
Lawn and Garden (16)	175	\$758.90	\$3,114,538	179	\$775.19	\$21,695,373	177	\$768.81	\$48,241,541
Moving/Storage/Freight Express	153	\$113.23	\$464,692	158	\$117.01	\$3,274,658	157	\$116.16	\$7,288,728
Housekeeping Supplies (17)	155	\$1,121.02	\$4,600,658	160	\$1,152.04	\$32,242,104	159	\$1,150.47	\$72,189,514
<b>Insurance</b>									
Owners and Renters Insurance	168	\$847.03	\$3,476,224	173	\$868.84	\$24,316,345	172	\$864.13	\$54,222,116
Vehicle Insurance	157	\$1,906.49	\$7,824,255	161	\$1,956.13	\$54,746,158	161	\$1,957.67	\$122,840,042
Life/Other Insurance	172	\$791.67	\$3,249,024	179	\$825.59	\$23,105,835	178	\$821.33	\$51,536,928
Health Insurance	163	\$4,307.12	\$17,676,425	166	\$4,382.81	\$122,661,660	165	\$4,355.51	\$273,299,267

**Table 20. Spending Potential Index (continued)**

	5-Minute Drive Time			10-Minute Drive Time			15-Minute Drive Time		
	SPI	Average Amount Spent	Total	SPI	Average Amount Spent	Total	SPI	Average Amount Spent	Total
Personal Care Products (18)	154	\$718.89	\$2,950,322	159	\$744.67	\$20,841,189	159	\$742.81	\$46,609,995
School Books and Supplies (19)	153	\$276.12	\$1,133,201	160	\$288.89	\$8,085,284	161	\$289.52	\$18,167,085
Smoking Products	128	\$596.52	\$2,448,120	128	\$597.58	\$16,724,465	128	\$596.21	\$37,410,721
<b>Transportation</b>									
Vehicle Purchases (Net Outlay) (20)	154	\$6,246.29	\$25,634,778	159	\$6,419.33	\$179,657,763	159	\$6,411.89	\$402,333,457
Gasoline and Motor Oil	149	\$5,238.05	\$21,496,953	153	\$5,373.04	\$150,375,301	153	\$5,370.45	\$336,985,114
Vehicle Maintenance and Repairs	160	\$1,790.04	\$7,346,324	165	\$1,836.57	\$51,399,953	164	\$1,830.16	\$114,838,718
<b>Travel</b>									
Airline Fares	179	\$857.01	\$3,517,153	186	\$890.00	\$24,908,471	186	\$888.28	\$55,737,788
Lodging on Trips	181	\$819.25	\$3,362,207	188	\$850.70	\$23,808,589	187	\$847.25	\$53,162,958
Auto/Truck/Van Rental on Trips	186	\$62.89	\$258,089	195	\$65.95	\$1,845,741	194	\$65.41	\$4,104,591
Food and Drink on Trips	173	\$807.50	\$3,313,980	179	\$834.87	\$23,365,447	178	\$830.93	\$52,139,262

The Spending Potential Index shows a significant amount spent on consumer spending categories compared to the national average. The spending index is slightly greater in the 0-10-minute and 0-15 minute areas as compared to the 0-5-minute market areas in almost every retail and service expenditure category. Areas of potential in New Britain Borough Commercial Area in terms of high SPI and/or high total spent include:

- Apparel and Services –Apparel Products and Services, SPI 157, \$14.9 million spent in the 0-5-minute market.
- Entertainment and Recreation – Fees and Admissions, SPI 181, \$4.8 million spent in the 0-5-minute market.
- Entertainment and Recreation – Pets, SPI 157, \$3.6 million spent in the 0-5-minute market.
- Food – Food at Home, SPI 152 \$32.6 million spent in the 0-5-minute market.
- Food – Food Away from Home, SPI 158, \$21.3 million spent in the 0-5-minute market.
- Financial – Investments, SPI 177, \$20 million spent in the 0-5-minute market.
- Home – Maintenance and Remodeling Services, SPI 184, \$12.7 million spent in the 0-5-minute market.
- Household Furnishings and Equipment – Furniture, SPI 161, \$3.4 million spent in the 0-5-minute market.

Caution must be given in using this data since consumers’ tastes and desires can change quickly in today’s retail environment. However, the data can still provide a glimpse of the retail potential in the three drive-time markets surrounding New Britain Borough Commercial Area.

#### RETAIL MARKETPLACE PROFILE

The retail marketplace profile is a snapshot of the supply and demand of retail sales in the 0-5-minute market surrounding the center of New Britain Borough Commercial Area. This analysis can assist in determining whether New Britain Borough Commercial Area is meeting local demand for products

in specific industry groups. Demand estimates the expected amount spent by consumers at retail establishments. The leakage/surplus factor is a measure of the relationship between supply and demand that ranges from +100 (total leakage) to -100 (total surplus). A positive value represents “leakage” of retail opportunity outside the trade area. A negative value represents a surplus of retail sales, a market where customers are drawn in from outside the trade area. The tables are limited to the 0-5-minute market, as this report is focused on businesses within the borough commercial areas.

**Table 21. Retail Leakage/Surplus Factors (0 to 5 Minute Drive-Time)**

Industry Summary	NAICS	Demand (Retail Potential)	Supply (Retail Sales)	Retail Gap	Leakage/ Surplus Factor	Number of Businesses
Total Retail Trade and Food & Drink	44-45,722	\$192,982,169	\$231,720,822	-\$38,738,653	-9.1	93
Total Retail Trade	44-45	\$173,496,623	\$225,402,759	-\$51,906,136	-13.0	80
Total Food & Drink	722	\$19,485,545	\$6,318,063	\$13,167,482	51.0	13
Industry Group	NAICS	Demand (Retail Potential)	Supply (Retail Sales)	Retail Gap	Leakage/ Surplus Factor	Number of Businesses
Motor Vehicle & Parts Dealers	441	\$34,858,754	\$5,634,451	\$29,224,303	72.2	4
Automobile Dealers	4411	\$30,658,063	\$0	\$30,658,063	100.0	0
Other Motor Vehicle Dealers	4412	\$1,934,292	\$3,710,887	-\$1,776,595	-31.5	1
Auto Parts, Accessories & Tire Stores	4413	\$2,266,400	\$425,073	\$1,841,327	68.4	2
Furniture & Home Furnishings Stores	442	\$3,885,822	\$1,529,453	\$2,356,369	43.5	7
Furniture Stores	4421	\$2,169,527	\$889,028	\$1,280,499	41.9	3
Home Furnishings Stores	4422	\$1,716,295	\$640,425	\$1,075,870	45.7	3
Electronics & Appliance Stores	4431	\$4,671,111	\$7,234,620	-\$2,563,509	-21.5	6
Bldg Materials, Garden Equip. & Supply Stores	444	\$6,473,236	\$2,401,004	\$4,072,232	45.9	8
Bldg Material & Supplies Dealers	4441	\$5,508,874	\$1,953,860	\$3,555,014	47.6	7
Lawn & Garden Equip & Supply Stores	4442	\$964,362	\$447,144	\$517,218	36.6	1
Food & Beverage Stores	445	\$30,888,147	\$15,385,530	\$15,502,617	33.5	7
Grocery Stores	4451	\$26,530,427	\$12,940,105	\$13,590,322	34.4	3
Specialty Food Stores	4452	\$856,870	\$897,158	-\$40,288	-2.3	3
Beer, Wine & Liquor Stores	4453	\$3,500,850	\$1,548,268	\$1,952,582	38.7	1
Health & Personal Care Stores	446,4461	\$9,932,513	\$7,238,078	\$2,694,435	15.7	6
Gasoline Stations	447,4471	\$15,885,269	\$9,577,667	\$6,307,602	24.8	2
Clothing & Clothing Accessories Stores	448	\$10,648,287	\$1,045,918	\$9,602,369	82.1	5
Clothing Stores	4481	\$7,973,208	\$907,154	\$7,066,054	79.6	4
Shoe Stores	4482	\$1,622,164	\$0	\$1,622,164	100.0	0
Jewelry, Luggage & Leather Goods Stores	4483	\$1,052,915	\$0	\$1,052,915	100.0	0
Sporting Goods, Hobby, Book & Music Stores	451	\$4,892,404	\$1,954,632	\$2,937,772	42.9	6
Sporting Goods/Hobby/Musical Instr Stores	4511	\$3,781,866	\$962,513	\$2,819,353	59.4	5
Book, Periodical & Music Stores	4512	\$1,110,537	\$992,119	\$118,418	5.6	2
General Merchandise Stores	452	\$24,719,872	\$24,895,159	-\$175,287	-0.4	4
Department Stores Excluding Leased Depts.	4521	\$10,865,512	\$4,358,591	\$6,506,921	42.7	1
Other General Merchandise Stores	4529	\$13,854,360	\$20,536,568	-\$6,682,208	-19.4	3
Miscellaneous Store Retailers	453	\$5,300,411	\$2,094,299	\$3,206,112	43.4	20
Florists	4531	\$301,693	\$0	\$301,693	100.0	0
Office Supplies, Stationery & Gift Stores	4532	\$1,510,931	\$194,332	\$1,316,599	77.2	4
Used Merchandise Stores	4533	\$537,717	\$379,177	\$158,540	17.3	4
Other Miscellaneous Store Retailers	4539	\$2,950,070	\$1,502,159	\$1,447,911	32.5	12
Nonstore Retailers	454	\$21,340,798	\$146,411,948	-\$125,071,150	-74.6	6
Electronic Shopping & Mail-Order Houses	4541	\$18,156,228	\$143,969,762	-\$125,813,534	-77.6	3
Vending Machine Operators	4542	\$470,729	\$112,979	\$357,750	61.3	1
Direct Selling Establishments	4543	\$2,713,841	\$2,329,207	\$384,634	7.6	3
Food Services & Drinking Places	722	\$19,485,545	\$6,318,063	\$13,167,482	51.0	13
Full-Service Restaurants	7221	\$8,481,041	\$3,569,019	\$4,912,022	40.8	5
Limited-Service Eating Places	7222	\$8,787,884	\$2,310,613	\$6,477,271	58.4	6
Special Food Services	7223	\$838,574	\$0	\$838,574	100.0	0
Drinking Places - Alcoholic Beverages	7224	\$1,378,046	\$376,372	\$1,001,674	57.1	1

There are a total of 93 retail trade and food and drink businesses within the 5-minute drive-time boundary of the business district. While there is no overall retail leakage or retail trade leakage there is leakage in the food and drink industries including, Full-Service Restaurants (\$4.9 million), Limited-Service Eating Places (\$6.4 million), Special Food Services (\$838,000), and Drinking Places – Alcoholic Beverages (\$1.0 million). Retail trade groups with leakages might fit well in a Main Street setting. Leakages exist for Furniture and Home Furnishing Stores (\$2.3 million), Clothing and Accessory Stores including clothing, shoe, jewelry, luggage, and leather goods stores (\$9.6 million), Sporting Goods, Hobby, and Musical Instruments Stores (\$2.9 million), Florists (\$301,000), Office Supplies, Stationery, & Gift Stores (\$1.3 million), Other Miscellaneous Store Retailers (\$1.4 million),

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## ASSETS, CONSTRAINTS, AND OPPORTUNITIES

The following summary lists assets, constraints, and opportunities described in this report, especially the Population and Demographic Trends, Land Use Characteristics, and Economic Development Conditions, that will affect future economic growth. Assets are the positive factors that exist within the corridor and that will form a foundation for future development. Constraints are the negative factors that will have to be overcome. Opportunities are the initial identification of potential possibilities toward an improved future.

### Assets:

- Stable residential neighborhoods near corridor
- Engaged business community
- Historic residential and church buildings
- Presence of Delaware Valley University and its student population
- Progressive and responsive municipal officials
- Two SEPTA stations in close proximity to corridor with service to Philadelphia via the Lansdale/Doylestown Regional Rail Line
- Vehicular connection to PA 611 and US 202 Parkway
- Significant population within 15 minutes of Borough
- Presence of the Town Center of New Britain Shopping Center
- High traffic counts along Butler Avenue
- Trail and pedestrian connections close by
- Redevelopment of Knoell property underway
- Proximity to Doylestown Borough and the Central Bucks region

### Constraints:

- Lack of unified streetscape and identifiable “Main Street”
- Lack of pedestrian amenities
- Lack of bicycle facilities/opportunities
- Lack of wayfinding to local attractions
- Fast vehicular traffic
- Pedestrian crossings inhospitable
- Inconsistent landscaping and lighting

- Lack of access management on some sites
- Missing connections between the corridor and regional trail network
- Disconnect between Delaware Valley University and the borough's businesses.

Opportunities:

- Redevelopment is already happening (Knoell property)
- Take advantage of other underutilized and vacant properties to implement vision
- Partner with Delaware Valley University to implement compatible development
- Take advantage of SEPTA stations
- Provide a civic space at the shopping center
- Continue marketing of the corridor and Borough
- Mixed Use Overlay Ordinance is in place

While there are some constraints that impact the corridor there are substantial assets and opportunities that provide positive conditions for its revitalization. The identification of these strengths will help form the basis for the Economic Development Strategies chapter.

# BUTLER AVENUE MAIN STREET REVITALIZATION PLAN

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The first part of this plan examined the existing conditions within the study area which included discussions on the population and demographic trends, land use characteristics, transportation conditions, and economic development conditions to establish potential opportunities within the study area. The second part of the plan, the Butler Avenue Main Street Revitalization Plan, establishes a vision for the corridor and, by way of recommendations and design solutions found within the Mobility Enhancement, Main Street Design, and Economic Development Strategies sections, the means to achieve this vision. The Land Use Plan then brings together the recommendations and design solutions from these sections to visually represent the vision in a series of segment plans. The final part of the Revitalization Plan, the Effective Action Plan, is to be used by the Borough as a guide to the plan's implementation.

## OVERALL VISION

Visioning is the process of developing consensus on the future of a community and then deciding what is necessary to achieve it. A vision statement captures what the members of the community most value about their community and the shared ‘ideal’ image of what they want their community to become. The vision statement creates a forward-looking strategic framework that gives municipal officials, community members, and stakeholders a comprehensive long-term perspective on what is necessary to make rational, tactical, and incremental decisions on community issues as they arise. The following is the overall vision statement for the Butler Avenue corridor; the vision statements for the Town Center, Historic Village, and University Village areas are found in the Land Use Plan section.

### Butler Avenue “Main Street” Vision Statement

*The Butler Avenue corridor has been the Borough’s “Main Street” since the Borough’s inception. Over time, there has been substantial physical and economic transformations; however, the corridor has reasserted its role as a significant regional economic center of activity.*

*By nurturing a thoughtful and effective planning approach, the corridor has reached its full economic and functional potential. Born out of a planning and regulatory process favoring flexibility and incentives, new development and redevelopment projects have created a well-balanced land use composition that satisfies the needs of business, employees, and patrons. New development and redevelopment projects have been undertaken that respect the residential neighborhoods and historic resources within the corridor and that further enhance the local character of the Borough. The economic prosperity of the corridor is due, in part, to ongoing efforts to capitalize on the synergy between the commercial, retail and service sectors and their unique relationship with Delaware Valley University.*

*Butler Avenue’s transportation network is safe and efficient for motor vehicles, bicycles, and pedestrians alike, with convenient access to transit stops. Connections to the regional trail network and to destinations within the Borough are present. The Butler Avenue Corridor has solidified its overall identity and sense of place through its well-conceived streetscaping, gateway features, and architectural guidelines.*

*Ongoing perseverance and due diligence from public interests, stakeholders, and municipal officials will allow this vision for the Butler Avenue corridor to flourish for future generations.*

## MOBILITY ENHANCEMENT

### CONNECTIVITY

One of the major issues identified through the public outreach process is to improve the walkability in the New Britain Borough. The lack of sidewalks, crosswalks, and insufficient road right-of-way have combined to prevent adequate connectivity within the study area. As noted in the Transportation and Mobility chapter, there are minimal sidewalks along Butler Avenue. In addition, the high volume and speed of vehicles creates unsafe conditions for any pedestrians interested in walking along Butler Avenue since there aren't many safe facilities for them.

There are several plans that have been developed with the goal of providing pedestrian and bicycle connections within the study area. A brief description of these plans is included below. The recommended sidewalks and trails from these plans are shown on Map 6 and have been considered in development of this study.

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#### *BUTLER AVENUE REVITALIZATION STRATEGIES (2009)*

In response to the construction of the US 202 Parkway, New Britain Borough along with Chalfont Borough and New Britain Township joined together to plan for the Butler Avenue corridor. The resultant study, *Butler Avenue Revitalization Strategies* (2009), included planning, visioning, and public outreach exercises to develop a conceptual streetscape and circulation plan for the corridor. Placemaking and transportation management techniques and improvements were identified to create a safer and more vibrant corridor and to support its transportation needs.

The study included a final conceptual plan that illustrates the recommend solutions for sidewalk extensions, crosswalks, and other improvements that will be discussed in other sections of this plan. The sidewalk recommendations for the study area are:

- Install missing sidewalk segments along the north side of Butler Avenue from Bristol Road to Tamanend Avenue.
- Install a sidewalk on the Pine Run Elementary School site from Butler Avenue to the building's entrance.
- Install crosswalks at the Butler Avenue's intersection with Lenape Drive, Tamanend Avenue, Iron Hill Road, and New Britain Road.
- Install crosswalk to connect the eastern end of the service road, Access B, in front of 125 West Butler Avenue (TMP #25-2-134).
- Construct missing sidewalk segment between the two service roads in front of 61 West Butler Avenue through 79 West Butler Avenue (TMPs #25-2-4, -5, -6, and -7).
- Construct a sidewalk on the west side of North Tamanend Avenue north of Butler Avenue.
- Sidewalks along the west side of South Tamanend Avenue from Butler Avenue south to New Britain Station.

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### NEW BRITAIN BOROUGH COMMUNITY BIKE AND HIKE MASTER PLAN

In 2009, New Britain Borough appointed a project study committee to develop the *New Britain Borough Community Bike and Hike Master Plan* with the goal of planning recreational trails in the Borough. The plan divides the proposed trail network into the Main Trunk Trail and spur trails.

The Main Trunk Trail would connect New Britain Road to Bristol Road and is divided into 4 sections. Section 1 of the Main Trunk Trail would connect New Britain Road to South Shady Retreat/Upper State Road via a trail that would follow Butler Avenue on the Delaware Valley University lands before following the existing alley adjacent to the University's athletic fields until the trail reaches South Shady Retreat Road. Section 2 connects South Shady Retreat Road to Beulah road via a recently built trail along the SEPTA rail line. Section 3 from Beulah Road to Tamanend Avenue would follow the north side of the SEPTA rail line. The plan indicates that there are two areas where slope and a stream crossing may impact trail construction. Section 4 from Tamanend Avenue to Sand Road would cross Tamanend Avenue at New Britain station and then follow Spring Lake Drive until the end of its cul-de-sac. A connector trail would traverse two properties until it reaches the cul-de-sac on Britain Drive. The trail would then continue until it reaches Sand Road.

The spurs include a "Commercial Greenway" that would follow the north side of Butler Avenue from Tamanend Avenue to Bristol Road. This trail would follow existing sidewalks and the service road islands. An alternate route is also discussed that would continue Section 4 of the Main Trunk Trail and follow the south side of the rail line from Sand Road to Bristol Road. Other spurs relevant to the study area include the South Tamanend Avenue spur which would connect the New Britain Station to Butler Avenue. This spur is slated to be constructed in 2017 with funding provided by the PennDOT Transportation Alternative Program (TAP). The Sand Road spur would connect Butler Avenue to Britain Road.

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### TRI-MUNICIPAL MASTER TRAIL AND GREENWAY PLAN

The *Tri-Municipal Master Trail and Greenway Plan*, also completed in 2009, is a cooperative effort among New Britain Borough, New Britain Township, and Chalfont Borough to plan a linked system of trails. Linkages in the borough include Butler Avenue, Neshaminy Creek at the Wilma Quinlan Nature Preserve, connecting west to Lenape Landing and Unami Middle School, and a linkage along the south side of Pine Run, connecting Covered Bridge Park to Forest Park to the west and Iron Hill Road to the east. Linkages are also shown along Sandy Ridge Road and Iron Hill Road where they connect to Butler Avenue and along Shady Retreat Road.

The proposed SEPTA Trail, which would connect Delaware Valley University to Bristol Road, aligns with the recommendations of the *New Britain Borough Community Bike and Hike Path Master Plan*. The committee that completed this study did extensive field work to define the preferred corridor. The route is parallel to the SEPTA rail line, crossing from the north to south sides at road crossings. A bridge will be needed to cross Cooks Run and a physical barrier will be required by SEPTA where the trail is located in its right-of-way. The trail connects to the Bristol Road Trail on the western end and

links to South Shady Retreat Road which extends east to connect to the Doylestown Community Bike & Hike System.

The *Tri-Municipal Master Trail and Greenway Plan* recommends that a pilot project be undertaken that would connect Covered Bridge Park to the Shady Ridge Road trail. This trail would traverse the western end of Covered Bridge Park, crossing the entrance drive with a crosswalk and extending as a multi-use trail adjacent to the east side of Keeley Avenue. The trail turns east onto Lamp Post Road as a separate multi-use trail and continues on the north and east side of Lamp Post Road until it intersects with East Butler Avenue. The trail is located on the north side of East Butler Avenue and continues to the east, turning east onto the north side of Shady Ridge Road, where it joins with the Doylestown Community Bike & Hike trail.

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#### BUCKS COUNTY BICYCLE MASTER PLAN

The *Bucks County Bicycle Master Plan, 2013* identifies one major bike connection that impacts New Britain Borough, the Neshaminy Greenway. The Neshaminy Greenway would establish a major northwest/southeast link across the county. This facility would link major population areas, such as Chalfont, Doylestown, Langhorne, Hulmeville, and Bensalem. It would also serve to connect various park facilities, including Neshaminy State Park and Tyler State Park. A final major benefit of a spine along the Neshaminy Creek would be the potential link/intersection with a variety of the planned on-road spines, as well as the Little Neshaminy Creek spine.

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#### PEDESTRIAN AND BICYCLIST SAFETY

Crosswalks and pedestrian countdown timers should be installed where needed to increase the safety of pedestrians at each of the intersections. Crosswalks alert drivers to the potential for pedestrians within the intersection. The pedestrian countdown timers show pedestrians how many seconds they have left to cross the road before the “don’t walk” symbol is displayed. This allows the pedestrian to make better decisions about whether to begin to cross the road. Both of these measures should improve the safety of the intersections for both motorists and pedestrians. The Streetscapes section presents several types of crosswalks including the continental design shown below. The specific style of crosswalk used at an intersection will depend on its location within the corridor.

**Figure 10. Continental Crosswalks and Countdown Pedestrian Timer**



There are also measures that can be taken to improve the safety of non-motorized traffic (bicyclists). Although there are some existing “Share the Road” signs posted within the study area, additional signage can be installed where appropriate to remind drivers that bicyclists may be on the road. Shared lane markings (or “sharrows”) could be installed along roadways with no shoulders and with speed limits below 35 mph to remind motorists that bicycle traffic may be present within the travel lanes. These markings also assist the cyclist with lateral positioning within the shared lane and the appropriate direction of travel. Bike Route Guide signs could also be implemented to inform bicyclists of the direction and distance to destination locations, such as Doylestown Borough, Delaware Valley University, or a dedicated bicycle route.

Any future roadway improvements and development in the study area should include pedestrian facilities. By striving to provide better connectivity in the study area, people would have the option of walking or biking to businesses. This would reduce congestion in the study area since fewer vehicles would be used for these trips and make the business area safer and more appealing.

Through the requirements in the subdivision and land development ordinance, New Britain Borough is able to ensure that new developments, both residential and nonresidential, have sidewalks or paths. The Borough ordinance requires sidewalks on all proposed and existing streets. This requirement should not be waived.

This study proposes the pedestrian and bicycle facilities shown on Map 6, along with the recommendations found in the New Britain Borough Bike and Hike Master Plan, the Butler Avenue Revitalization Strategies report, and the Tri-Municipal Master Trail and Greenway Plan. The Borough, through the subdivision and land development process, can require trail segments shown on these plans to be built or easements provided for future construction of paths and trails.

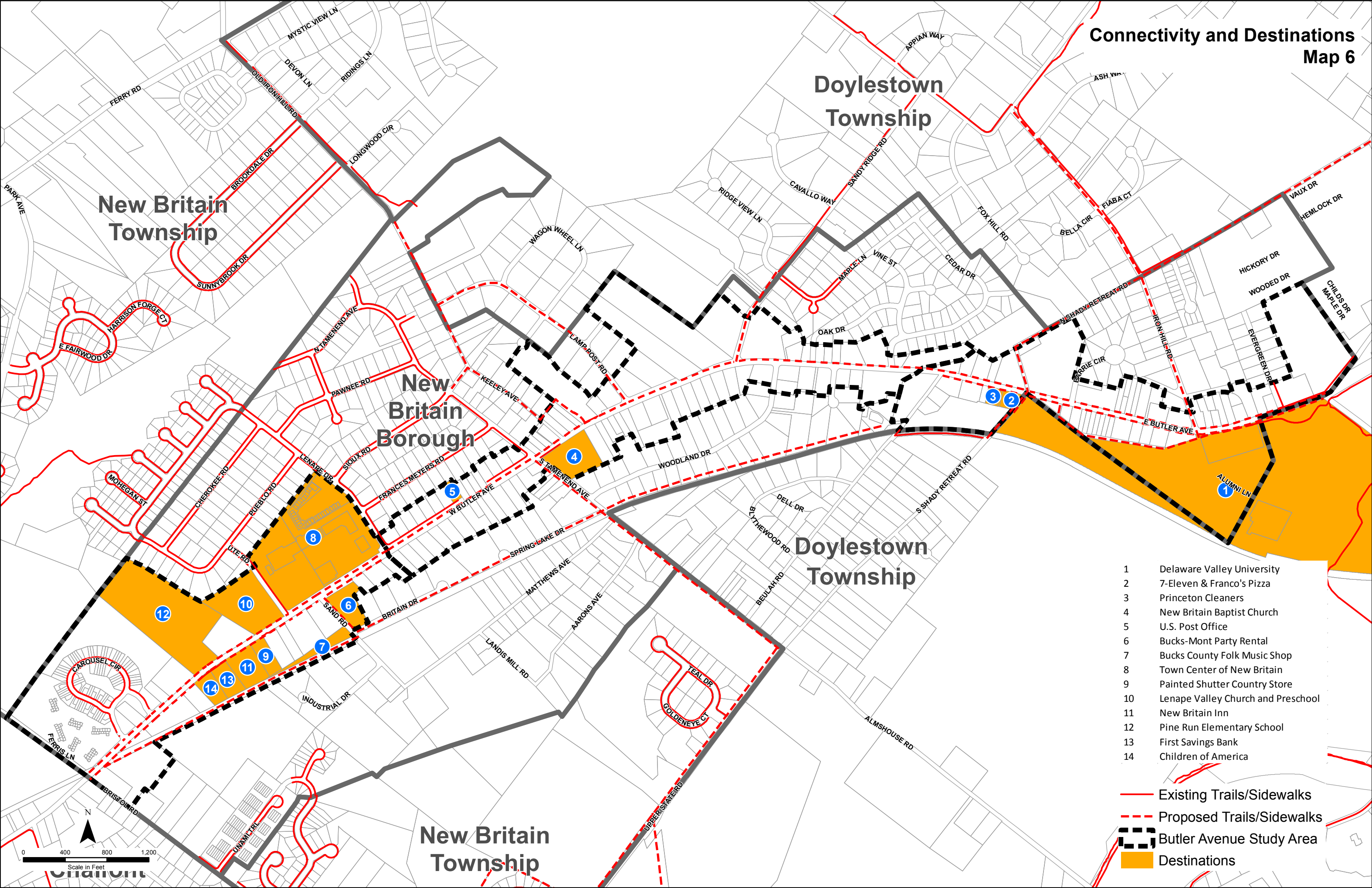
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## TRANSPORTATION SYSTEM

The function of the transportation system is to provide for the movement of people and goods between places. The adequacy of this system is directly influenced by the type and size of the population growth and land development that occur along the network. Where appropriate, transportation improvements should be designed as multiple-use facilities that provide for pedestrians, bicycles, public transit, and motor vehicles. As the borough maintains and upgrades its roads, a balance among parking needs, local vehicle movements, and emergency vehicle movements must be upheld. The livability of neighborhoods and the historic character of the borough should also be considered as improvements are made to the road network.

The continued development and redevelopment of the borough depend on the advantages that a well-functioning transportation system provides. Maintaining and enhancing the quality and efficiency of the system will support the borough’s economic development efforts.

Connectivity and Destinations  
Map 6



One potential solution to the congestion that occurs at intersections along the corridor is the installation of a roundabout. A roundabout is a circular intersection where vehicles travel counterclockwise around a center island. There are no traffic signals or stop signs in a roundabout. Drivers yield at entry to traffic in the roundabout, then enter the intersection and exit at their desired street.



Studies by the Federal Highway Administration have found that roundabouts can increase traffic capacity by 30 percent to 50 percent compared to traditional intersections. In addition, roundabouts have been shown to reduce injury crash rates by 75 percent where traffic signals were previously used. They have the added benefit of reduced maintenance and costs since traffic signals are eliminated. Widening of the roadway or realigning the intersecting roads would not be necessary since the turn lanes and a right-angle alignment would no longer be needed. As the borough addresses congestion issues along the corridor, roundabouts should be included in the list of potential solutions for each intersection.

**Figure 11. Roundabout Example**



## ACCESS MANAGEMENT

When access is not controlled, the number of conflict points<sup>11</sup> with roadway traffic increases. This places serious demands on the roadway capacity, as well as making conditions unsafe for vehicles entering or exiting the highway. Controlling the number of access points allows roads to perform their intended function.

Access management is both a land use and traffic issue. It calls for land use controls and incentives that are keyed to the development policies of the community and the capabilities of the transportation system. The planning challenge is how to provide driveways or parking to accommodate new development while expediting traffic flow. The Borough must take into account the access requirements of businesses that may relocate into the area, as well as those vehicles traveling through the area. Access specific management techniques include shared driveways and parking, providing access to secondary roadways, driveway spacing, planted median strips, and protected left turn lanes.

Butler Avenue contains several parcels that do not have a defined driveway. Several forms of access management are appropriate for the area. Providing a sidewalk in these areas with landscaping will ensure that access to and from these parcels is safer for pedestrians. The addition of curbing with planted islands to denote the location of the driveways would improve safety for vehicles entering and exiting each site. The planted islands also provide the additional benefit of acting as a streetscape improvement, which is an aesthetic benefit for the corridor. A photosimulation is shown in Figures 12 and 13 using an actual site within the corridor to demonstrate access management combined with streetscape improvements.

New development or redevelopment will have to meet the access management requirements of the Borough's subdivision and land development ordinance. The Borough should confirm that this ordinance contains up-to-date standards on driveway widths, driveway separations, required curbing and landscaping, opportunities to encourage shared driveways, and distances between driveways and streets. Implementing access controls for already developed sites will require either the cooperation of the property owner to make changes voluntarily or the investment of public or grant funds for streetscape improvements that can be made with property owner permission and involvement.

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<sup>11</sup> A conflict point is a place where two vehicles come together or their paths cross and one or both drivers must take evasive action to avoid collision.

Figure 12. Sample Site with a lack of Access Management



Figure 13. Sample Photosimulation of Potential Access Management Techniques



## STREETSCAPE IMPROVEMENTS

In conjunction with the addition of sidewalks to aid pedestrian safety, streetscape improvements should be included in any roadway improvements or land developments. Aesthetic issues such as lighting, signage, and pedestrian amenities are important components of streetscape planning within the community. Streetscape improvements are an integral part of revitalization of the study area. Streetscape improvements combine roadways with sidewalks, signage, lighting, landscaping and other elements in the public right-of-way to create public space for pedestrians and vehicles. Specific streetscape recommendations are found in the Design for Main Street section.

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## SPEED REDUCTION

The Central Bucks Regional Police Department's 2014 Traffic Study revealed that residents' perception that traffic is moving at a high rate of speed through the corridor is true. Throughout the planning process concerns have been raised about how the types of development envisioned for the corridor would fit and if pedestrians would feel comfortable and be safe given the rate of speed observed. The Pennsylvania Department of Transportation, in conjunction with the New Jersey Department of Transportation, have produced the *Smart Transportation Guidebook* (2008), which cites evidence on the ways that different design elements reduce speeds. Many of these elements are proposed as part of the vision for the corridor. A brief discussion of these design elements and how they affect vehicle speeds taken from the Smart Transportation Guidebook are found on the following page.

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## POTENTIAL FUNDING SOURCES

Any improvements to circulation in the Borough will involve some cost. However, these costs can be typically offset through a variety of funding sources. The list found in Appendix 3 describes potential funding sources and requirements of grant programs. Programs and funding sources change frequently, and other grants may be available.

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## SUMMARY OF MOBILITY ENHANCEMENT STRATEGIES/RECOMMENDATIONS

Proper land use planning is important to prevent adverse effects to the transportation network due to improper development. When transportation improvements are designed, it is important to address the needs of the general public, individual property owners, and neighborhoods. Transportation improvements should be designed as multiple-use facilities that provide for pedestrians, bicycles, public transit, and motor vehicles. The "multi-modal" approach to providing facilities for all types of travelers has become a fundamental concept in transportation planning and transportation funding. As the Borough maintains and upgrades its roads, a balance among parking needs, local vehicle movements and emergency vehicle movements must be made. The livability of neighborhoods and the historic character of the Borough should also be considered as improvements are made to the road network.

## DESIGN FEATURES THAT AFFECT OPERATING SPEEDS

**Street Trees** — Street trees in planting strips appear to have a traffic calming benefit by causing the motorist to believe the space is tighter and more restrictive.

**Lane Widths** — Narrower lane widths are associated with lower speeds. One study of suburban arterials found that, once posted speeds are discounted, lane width is the only significant variable for operating speeds on straight sections. A relationship between lane widths and speeds was also identified in a study of urban collector roadways in central Pennsylvania. It should be acknowledged that other studies have found no relationship between lane width and speeds.

**Total Roadway Widths** — Narrower roadway widths are associated with lower operating speeds.

**Access Density** — Higher density of access points is associated with lower operating speeds. Along roadways with uncontrolled access, drivers must be vigilant to interaction with driveways, intersections, median areas, and parking.

**Signal Density** — Higher signal density is associated with lower operating speeds. In their recommendations for signal progression, particularly on roadways with closely spaced signals, engineers have great influence on the speed of prevailing traffic.

**On-Street Parking** — On-street parking leads to lower speeds, due to side friction between moving and passing vehicles. One study found that on otherwise similar roadways, speeds were 7.5 mph lower on roadways with parked cars.

**Curbs** — Speeds appear to be lower on streets with curbs than streets without curbs, although one study found no relationship between speeds and the presence of curbs.

**Pedestrian Activity** — Speeds are lower on roadways with higher pedestrian activity.

**Roadside Development** — Speeds are lower in residential areas than commercial areas. Building setback also matters. As part of an effort to quantify “Main Streetness” for Flexible Design of New Jersey’s Main Streets, building setback from the street was determined as one of the five key variables in whether people perceive a roadway to be a “Main Street.”

**Physical Traffic Calming Measures** — The ability of traffic calming measures to lower vehicular speeds has been well documented. Nationwide, speed humps and mini-circles are the most popular measures on lower order roadways. On higher order roadways, less intrusive measures, such as curb extensions or roundabouts should be considered.

*Source: Smart Transportation Guidebook, New Jersey & Pennsylvania Departments of Transportation (2008)*

The continued development and redevelopment of the Borough depends on the advantages that a well-functioning transportation system provides. Maintaining and enhancing the quality and efficiency of the system will support the Borough's economic development efforts along Butler Avenue. The following are strategies and recommendations for study area:

1. Investigate the possible installation of a roundabout at Butler Avenue and Shady Retreat Road;
2. Improve the pedestrian circulation system through improvements to the sidewalk and path system and pedestrian crossings at street intersections;
3. Ensure that access management techniques are implemented as development occurs within the Borough;
4. Ensure that streetscape techniques are implemented as development occurs within the Borough;
5. Provide bike trail connections from the Borough to schools, parks, and community facilities and other trails;
6. Increase public/private sector cooperation to foster arrangements for shared parking for businesses within the Borough and support with ordinance incentives; and
7. Work with SEPTA to ensure that the public transportation continues to provide service to residents and businesses.

## MAIN STREET DESIGN

The vision for Butler Avenue is for it to both function and to have the appearance of a “Main Street.” This section provides a series of design solutions to implement the vision and create a “Main Street” with a sense of place within the study area. The elements of the Design for Main Street are provided for in the Streetscapes and Design Guidelines discussions. Illustrations and photosimulations are used as representative designs of the proposed improvements.

### STREETSCAPES

Streetscaping is an important component of the overall transportation system forming a connected aesthetic corridor to help facilitate revitalization in the study area. A streetscape analysis looks at multiple aspects in conjunction with proposed transportation and roadway improvements and upgrades. It investigates the space where vehicles and people interact, both visually and physically. The primary goal is to establish a safe boundary for pedestrians to navigate the vehicular corridor, preferably within the public street right-of-way. In turn, it promotes walkability and can improve the overall visual curb appeal through the use of uniform amenities and landscape features.

The New Britain Borough Community and Economic Development Initiatives (April 2014) and the Town Hall meeting on March 5, 2013 identified streetscape (sidewalks and trees along Butler Avenue), pedestrian connections, walkability, and gateways as potential attributes to help improve New Britain Borough’s future character and business environment. Improving these components along with other transportation and roadway recommendations will complement the revitalization initiatives and help create a sense of identity and place for the Borough with the primary vision to create a prototypical ‘main street’ type environment along Butler Avenue.

In New Britain Borough’s case, the streetscape design is context sensitive and differs for each segment of Butler Avenue. Butler Avenue’s characteristics change in surrounding uses and functions, as well as aesthetics along the study area span. The following narrative discusses determinants of streetscape function (user groups, connectivity, and destination) and preferred design solutions and identifies each street segment type and summarizes amenity design and landscaping recommendations that apply to each streetscape type. The three segments are, in order from west to east:

Segment Name	Segment Limits
Town Center	Bristol Road to Lenape Drive
Historic Village	Lenape Drive to Beulah Road
University Village	Beulah Road to New Britain Road

Gateway features are also identified at strategic locations to signify major entry points and locations within the study limits. A gateway could be positioned at each end of the study along Butler Avenue and internally at the extents of the University Village area at Delaware Valley University.

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*USER GROUPS, CONNECTIVITY, AND DESTINATIONS*

The study area itself and the immediate surrounding residential neighborhoods create a strong user group foundation. The market population within the 5-minute drive time is projected to remain constant until 2018 at approximately 11,500 people and the age brackets of 15-24 and 35-54 will continue to be highest numbers living within the 5-minute drive area. The proximity of Delaware Valley University (DVU) factors in with a student population over 2,000 full- and part-time students and approximately 700 employees. By creating a safe, walkable main street, these user groups will be the ones most likely to shop, exercise and save on driving costs.

Within the study area, sidewalks are limited along both East and West Butler Avenue. However, the neighborhoods surrounding the Town Center of New Britain shopping center have sidewalks as well as a span along West Butler Avenue that provides a connection into the shopping area. Several adjacent neighborhoods on either side of the corridor that have street connections do not have direct sidewalks that tie into Butler Avenue, thereby diminishing the potential of people walking over driving. Sidewalk links from older neighborhoods should ultimately be studied and proposed development within the study area should have sidewalks or trails proposed. There is a network of existing trails and sidewalks connecting with and through DVU as well as a direct connection into Doylestown Borough. Map 6 shows the sidewalks and trails within the study area.

The main street corridor has several destinations throughout its entire span (see Map 6). It is flanked by two major destinations, Delaware Valley University to the east and the Town Center shopping center to the west. In addition to the shopping center and DVU, other potential primary walking destinations within the study area include, and are not limited to:

- 7-Eleven & Franco's Pizza
- Princeton Cleaners
- New Britain Baptist Church
- U.S. Post Office
- Bucks-Mont Party Rental
- Bucks County Folk Music Shop
- Painted Shutter Country Store
- Lenape Valley Church and Preschool
- New Britain Inn
- Pine Run Elementary School
- Penn Community Bank
- Children of America

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### PREFERRED DESIGN SOLUTIONS

Without reconstructing a main street environment from scratch, these adaptive guidelines are intended to modify and adapt the current existing conditions to recreate a “Main Street” type environment as much as possible within the study area.

The design solutions noted below are preferred and recommended and not always feasible or practical due to the existing conditions. They are based upon current standard practices<sup>12</sup> and, where possible, they can be used as a guide, or as redevelopment of tracts occur, they can be implemented. As further development and redevelopment occurs, it is recommended that streetscape elements and gateway features (where proposed) be incorporated into the design during the land development process. Seeking funding through grants to design and construct sidewalks/paths and other streetscape elements is recommended.

#### SIDEWALKS

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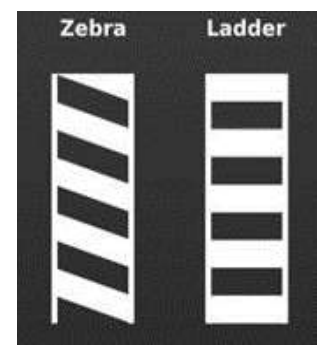
- Sidewalks shall be designed to facilitate comfortable, barrier-free walking
- Sidewalk Location: Prefer both sides of streets; require on at least one side\*
- Sidewalk Width: Minimum 4 to 5 feet, comfortable for two people to walk side by side\*
- Where there is no planting strip, sidewalk width shall be 5 feet minimum\*
- Where possible, should be accessible to a plaza area or public, civic green
- Sidewalks shall be laterally pitched toward the street at a slope not less than 1/4 inch per foot to provide for adequate drainage\*

\*Coincides with subdivision and land development ordinance

#### STREET CROSSING/INTERSECTION DESIGN

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- Limit to existing intersections with traffic signals, avoid mid-block crossing unless traffic speed can be reduced
- Reduce crosswalk length if possible
- Provide ADA accessible crosswalk ramps
- Crosswalk widths shall be not more than 10 feet and not less than 6 feet with borders highlighted with white lines at least 6 inches in width
- Provide pedestrian ‘walk’ signals with push-button mechanisms
- Crosswalks shall be constructed of inlaid thermal plastic, patterned surface dressing, or stone/concrete pavers that are safe and easy to view made with different colors or textures to distinguish them; white lines painted in a prominent style, such as zebra or ladder styles are acceptable
- Provide lighting at crosswalks
- Provide proper, adequate sight distance for vehicles at corners
- Signs, advance pavement markings, or flashing lights to warn motorists of an upcoming crosswalk
- Consider flashing signals and lights and advanced warning signs to increase the visibility of the crosswalk



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<sup>12</sup> Manual of Uniform Traffic Control Devices (MUTCD)

#### DRIVE AND DRIVEWAY CROSSINGS

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- Crosswalk lines shall be painted white in a prominent style, such as zebra or ladder styles
- Crosswalk widths shall be not less than 6 feet with borders highlighted with white lines at least 6 inches in width

#### RAISED MEDIAN ISLAND

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- Preferred width of 6 feet wide; however, width may be reduced to 4 feet when existing street cartway warrants a narrower width or per recommendation of municipal governing body or reviewing agency
- Preferred length of 20 feet; however, length may be reduced to 12 feet when a longer length would interfere with a driveway or per recommendation of municipal governing body or reviewing agency
- Portions of raised median islands not used for sidewalk shall be landscaped with low maintenance, hardy, non-water intensive species
- Planted area shall be grass or evergreen ornamental groundcover not to exceed 18 inches in height

#### LANDSCAPING AND GREENING

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- For ease of pedestrian comfort (due to the speed limit of 40 mph along Butler Avenue) a planted verge/strip between the street and sidewalk shall be established
- Minimum width of planting strip shall be 2 feet; however 3 feet or greater is preferred and should be maintained between edge of sidewalk and top of curb or cartway
- Planted verge/strip shall be grass or evergreen ornamental groundcover not to exceed 18 inches in height
- Plant street trees if space allows
- Street trees shall be a minimum of two-and-one-half-inch caliper and at intervals not to exceed 40 feet\*
- Street trees shall be limbed up 8.5 feet minimum for pedestrian and bicycle clearance
- Low maintenance, hardy, non-water intensive species
- For plaza areas: at least 25 percent, but no more than 80 percent shall be landscaped with a mix of trees, shrubs, and groundcovers with year round interest
- Open space and plaza areas shall be ADA accessible for public access

\*Coincides with subdivision and land development ordinance

#### LIGHTING

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- Conform with ordinance requirements
- Pole fixture shall be at least 20 inches (horizontal separation) from top of curb
- Lighting for sidewalk and shopfront areas is most effectively provided by pedestrian-scale streetlights (12 to 15 feet in height). Spaced about 60 feet apart on ornamental poles, they can also provide roadway lighting for streets less than 45 feet wide

- Not to interfere with street trees
- Outdoor illumination shall be diffused or shielded in such a manner as not to create any hazardous situations for passing vehicular traffic or a nuisance to persons residing in the area\*
- Light fixtures shall be constructed of durable, attractive materials and be easy to maintain (cast iron, aluminum, stainless steel or approved similar)
- Provide pedestrian-scaled lighting (William & Mary style luminaire and Edgewater/West Point lamp post by Spring City Electrical)\*\*

\*Coincides with zoning ordinance

\*\*Full specification included in Appendix 4



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#### SIGNAGE AND BANNERS

- Conform with ordinance requirements
- Create a uniform, cohesive identity in form and materials
- Minimum height bottom of banner shall be 8.5 feet above grade line

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#### STREET FURNITURE AND PUBLIC ART

- Street furniture shall be provided and include (though not limited to) benches, waste and recycling receptacles, bike racks, and planters
- Strategically located (not to impede free passage) and properly scaled to the space
- Style and color specifications shall be approved by Borough
- Where adequate space allows; offset from curbs at a safe distance
- Potentially incorporate into gateway areas with greening elements
- Incorporate into public, civic green, and University Village plaza areas in combination with greening elements

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### STREETSCAPE SEGMENTS

The streetscape segments are classified in a tabular format on the following page. Each streetscape element is listed with recommendations for improvement tailored to each varying streetscape segment context. The tables contain generic as well as specific recommendations providing the guiding means to create a unified streetscape corridor. These recommendations/opportunities are intended to adapt as new construction occurs or new treatments and techniques are developed.

## TOWN CENTER (BRISTOL ROAD TO LENAPE DRIVE)

Streetscape Element	Recommendation/Opportunity
<b>Sidewalks: Pedestrian Mobility</b>	Provide sidewalks or macadam trail - connect to shopping center - widths to vary - both sides
<b>Street Crossing/Intersection Design Improvements</b>	Sand Road/West Butler Avenue and Bristol Road/West Butler Avenue intersection and access drive crossings
<b>Landscaping and Greening: Street Trees &amp; Planting Strips</b>	Incorporate street trees and ornamental trees under overhead utilities
<b>Bicycle Mobility</b>	Bike racks in shopping center and potential alternative route with Sand Road spur <sup>13</sup> (Sharrows along Sand Road)
<b>Lighting</b>	Pedestrian scaled, period-type fixture style with arms for banners
<b>Identity: Wayfinding Signs and Banners</b>	Standardize street signs
<b>Identity: Gateway Feature</b>	Public green on shopping center site and incorporate gateway feature at Borough entrance at Bristol Road intersection
<b>Street Furniture and Public Art</b>	Decorative: bollards, benches, trash receptacles
<b>Parking/Vehicular Control/Traffic Calming</b>	Incorporate parking in rear of nonresidential building lots
<b>Open Space/Mini-Plaza</b>	Public green on shopping center site and provide mini-plaza with seating (if possible)
<b>Building: Dimensional Control</b>	'Build to line' along street frontage between Lenape Drive and Ute Road and maintain current setbacks from Ute Road to Bristol Road

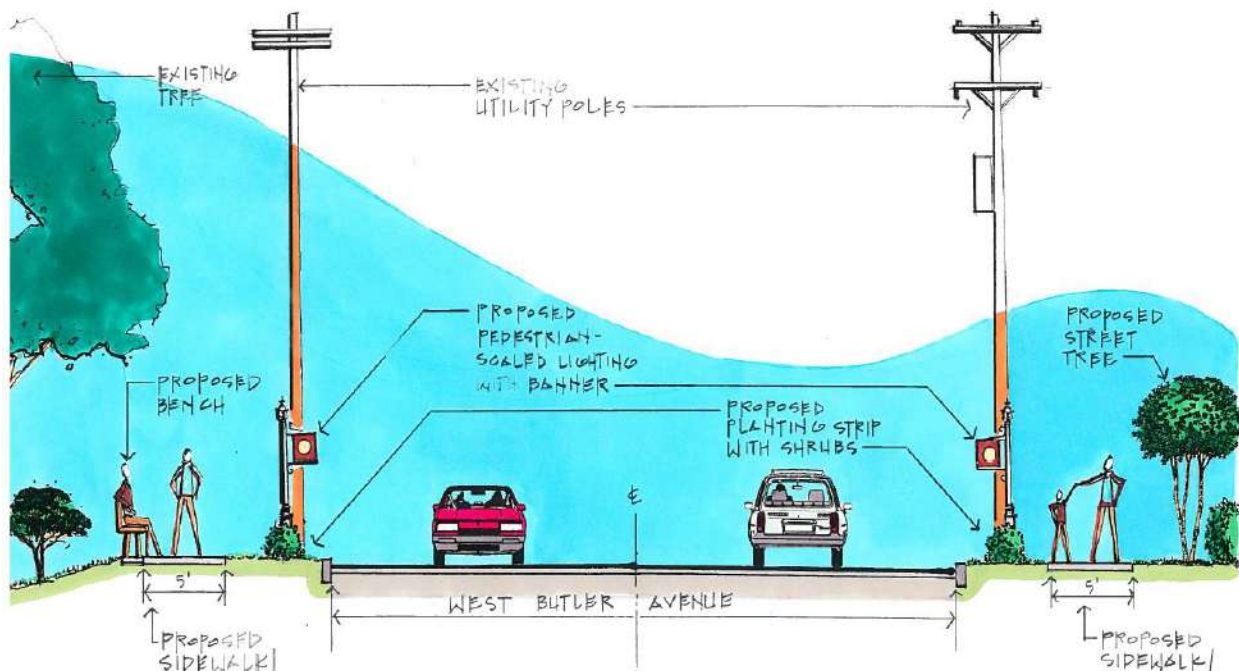
<sup>13</sup> New Britain Borough Community Bike and Hike Path (7/09)

## DESIGN SOLUTIONS

- Elements and materials added will complement themes associated with the Town Center
- Street frontage mobility should include new walkways (sidewalk/macadam) connecting with existing sidewalks along Lenape Drive and Ute Road
- Incorporate new pedestrian walkway spines (sidewalk/macadam) linking the street frontage, new retail area or public spaces, and the existing retail areas
- ADA compliant curbs and crosswalks to provide connections across access drives and at Sand Road and West Butler Avenue intersection
- Maintain street visibility of existing shopping center uses

The following illustration, Figure 14, depicts a typical streetscape cross-section within the Town Center segment. The illustration incorporates the common streetscape elements recommended for the segment and is intended to represent what the Town Center could become. As development and redevelopment occurs, a mixture of the streetscape elements and design solutions will be employed to create the desired “Main Street” character.

**Figure 14. Typical Streetscape Cross-section within Town Center segment**



Figures 15 and 16 below depict the existing conditions and the streetscape improvements proposed for the street frontage along the Town Center of New Britain shopping center. Elements depicted include sidewalks, street trees, and crosswalks. Also shown are enhancements to the lawn area at the southwest corner of the shopping center, which is discussed further in the Land Use Plan.

**Figure 15. Town Center Existing Streetscape Conditions**



**Figure 16. Town Center Streetscape Improvements**



## HISTORIC VILLAGE (LENAPE DRIVE TO BEULAH ROAD)

Streetscape Element	Recommendation/Opportunity
<b>Sidewalks: Pedestrian Mobility</b>	Provide sidewalks or macadam trail - one side minimum – widths to vary – connect with historic resources, where feasible based on topography and terrain
<b>Street Crossing/Intersection Design improvements</b>	Potential crossing at signal at South Tamanend Avenue/West Butler Avenue intersection (NW side) and at driveway crossings
<b>Landscaping and Greening: Street Trees &amp; Planting Strips</b>	Incorporate street trees and ornamental trees under overhead utilities
<b>Bicycle Mobility</b>	Alternative route with South Tamanend Avenue spur <sup>14</sup> (Sharrows along South Tamanend Avenue)
<b>Lighting</b>	Strategically identify location for pedestrian scaled, period-type fixture style with arms for banners and upgrade existing overhead lighting to match style and color
<b>Identity: Wayfinding Signs and Banners</b>	Standardize street signs and provide arm for municipal banners on existing utility poles
<b>Identity: Gateway Feature</b>	Incorporate markers at specific locations or extent of segment area
<b>Street Furniture and Public Art</b>	If possible - decorative: bollards, benches, trash receptacles strategically located
<b>Parking/Vehicular Control/Traffic Calming</b>	Incorporate parking in rear of nonresidential building lots
<b>Open Space/Mini-Plaza</b>	Provide mini-plazas with seating (where appropriate)
<b>Building: Dimensional Control</b>	Maintain current setbacks except where historic development pattern exists, then historic pattern should continue

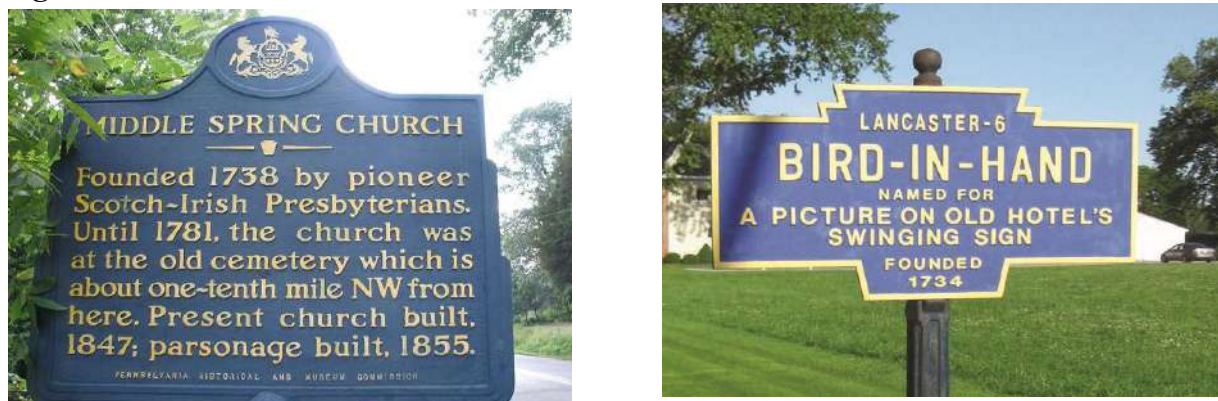
<sup>14</sup> New Britain Borough Community Bike and Hike Path (7/09)

## DESIGN SOLUTIONS

- Provide sidewalk or macadam trail to connect with the Town Center and University Village segments
- ADA compliant curbs and crosswalks to provide connections across Access B and at Tamanend Road and Butler Avenue intersection
- Provide wayfinding signs to identify destinations within the segment
- Maintain historic development patterns and preserve historic resources

Typical historic markers are shown below in Figure 17. Signs like these could be installed at the entrances to the Historic Village segment to emphasize the historic nature of the area.

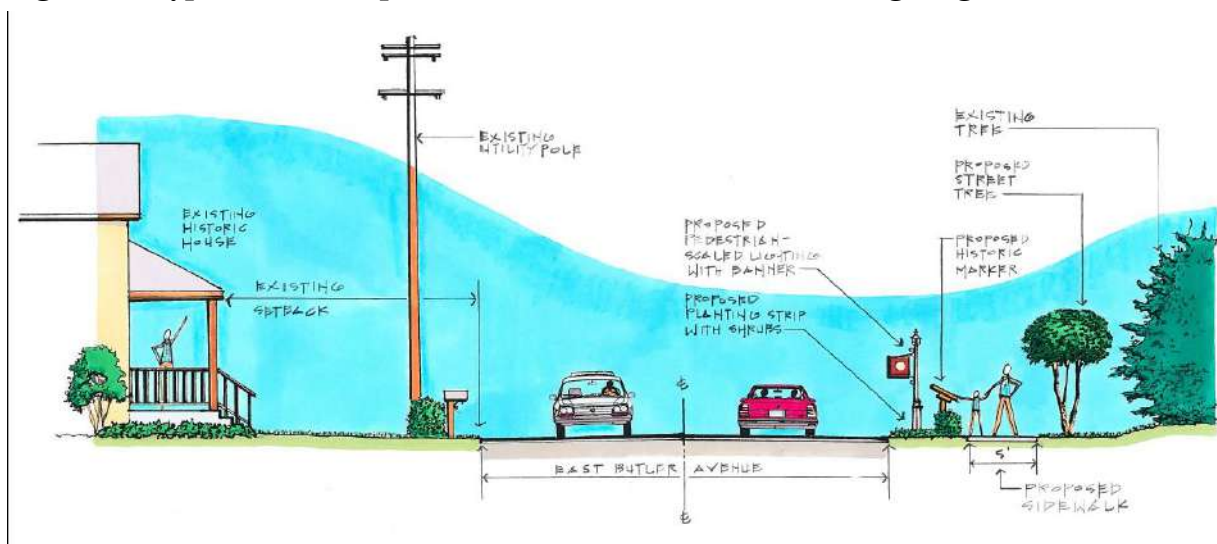
**Figure 17. Historic Markers**



Typical Markers to signify a person, place, or event.

Figure 18 depicts a typical streetscape cross-section within the Historic Village segment. The illustration incorporates many of the streetscape elements recommend for the segment and is intended to represent what the Historic Village could become. Significant changes are not proposed for this segment. Enhancements that emphasize the historic character of the segment and pedestrian facilities to connect with the other segments are the primary proposals.

**Figure 18. Typical Streetscape Cross-section within Historic Village segment**



UNIVERSITY VILLAGE (BEULAH ROAD TO NEW BRITAIN ROAD)

Streetscape Element	Recommendation/Opportunity
<b>Sidewalks: Pedestrian Mobility</b>	Provide sidewalks connected to University and existing trail network - widths to vary - both sides
<b>Street Crossing/Intersection Design Improvements</b>	Alumni Lane/East Butler Avenue intersection* and Shady Retreat Road and East Butler Avenue
<b>Landscaping and Greening: Street Trees &amp; Planting Strips</b>	Incorporate street trees and ornamental trees under overhead utilities
<b>Bicycle Mobility</b>	Bike racks and links to existing trails established in Delaware Valley University and create trail along south side of South Shady Retreat Road <sup>15</sup>
<b>Lighting</b>	Pedestrian-scaled, period-type fixture style
<b>Identity: Wayfinding Signs and Banners</b>	Incorporate into streetscape
<b>Identity: Gateway Feature</b>	Incorporate two gateways: Eastern Gateway at New Britain Road and convergence of East Butler Avenue/State Street intersection and Western Gateway at Beulah Road and East Butler Avenue intersection
<b>Street Furniture and Public Art</b>	Decorative: bollards, benches, trash receptacles
<b>Parking/Vehicular Control/Traffic Calming</b>	Parallel parking along East Butler Avenue and incorporate shared parking and alleys in rear of lots; and add landscaped islands in strategic locations along East Butler Avenue to promote access management for excessively wide driveways along the corridor
<b>Open Space/Mini-Plazas<sup>16</sup></b>	Provide mini-plazas/pocket parks with seating
<b>Building: Dimensional Control</b>	'Build to line' along street frontage

<sup>15</sup> New Britain Borough Community Bike and Hike Path (7/09)

<sup>16</sup> One other notable area to coordinate and integrate with the streetscape, intersection design, and Delaware Valley University entrance is the lot on the western side of Iron Hill Road across from Alumni Lane. It is recommended to be open space or a pocket park and the design intent of this area is to be compatible in scale, materials and size with any new entrance treatments into Delaware Valley University.

## DESIGN SOLUTIONS

- Elements and materials added will signify and complement themes associated with the Delaware Valley University entrance
- Consider traffic control measures (signage and signalization)
- ADA compliant curbs and crosswalks to provide connections
- Low-growing ornamental landscaping to complement existing entrance plantings

The following illustration, Figure 19., depicts a typical streetscape cross-section within the University Village segment. The illustration incorporates many of the streetscape elements recommend for the segment and is intended to represent what the University Village could become. As development and redevelopment occurs, a mixture of the streetscape elements and design solutions will be employed to create the desired “Main Street” character.

**Figure 19. Typical Streetscape Cross-section within University Village segment**

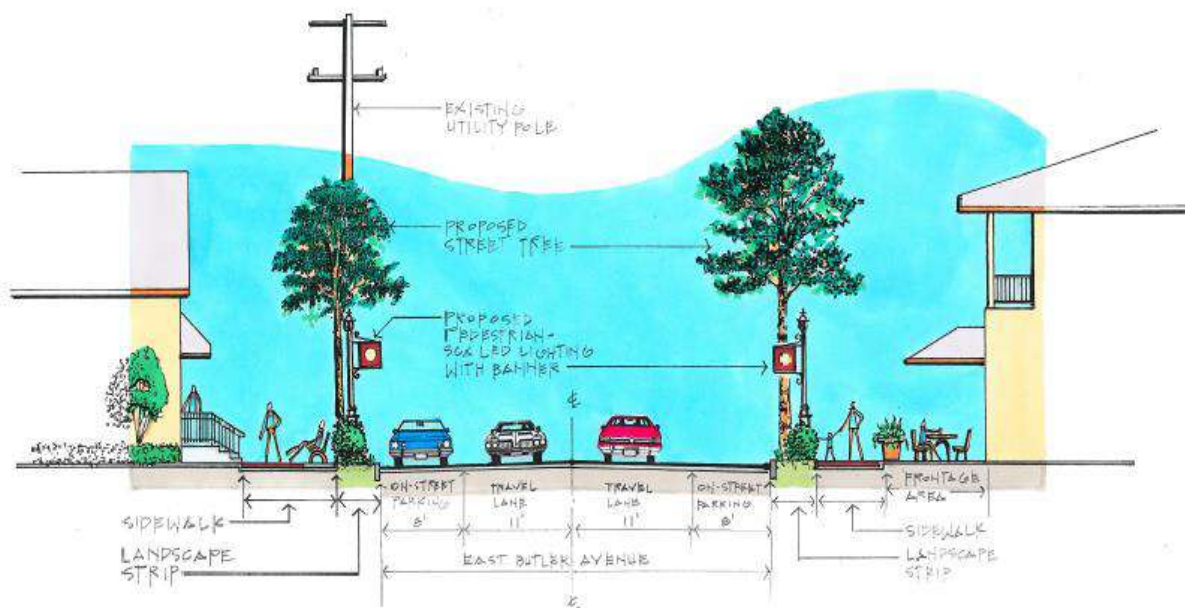
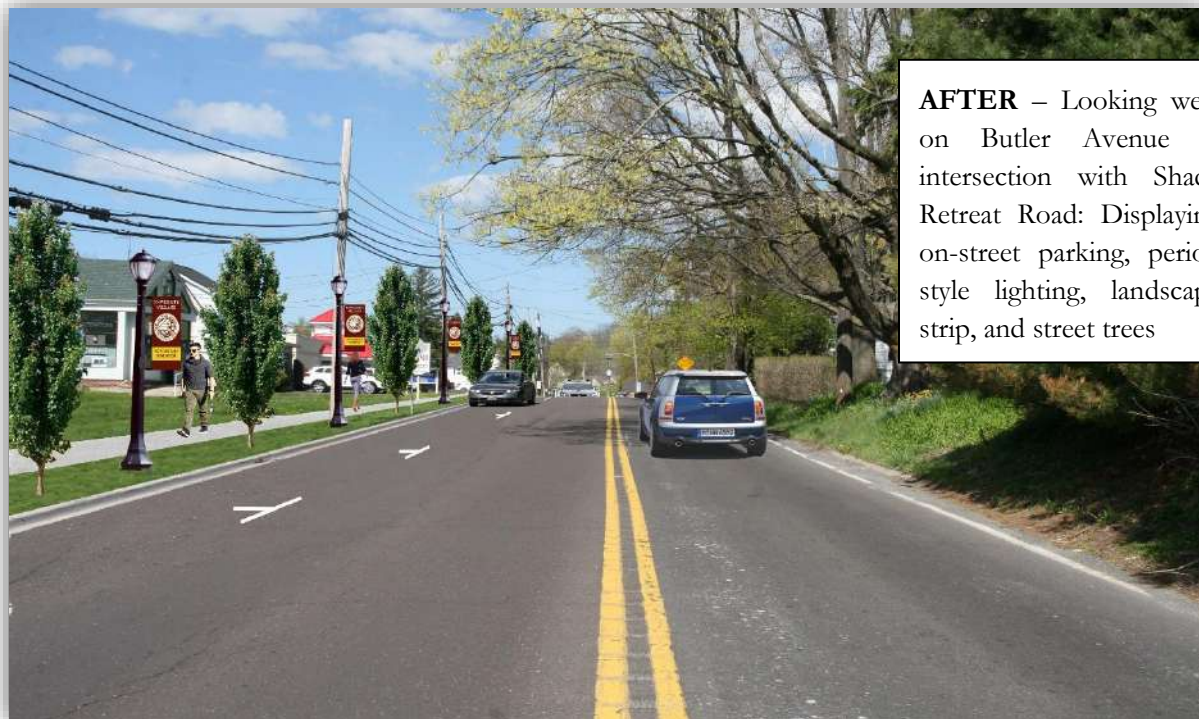


Figure 20 is a photosimulation created to illustrate a potential streetscape improvement. Located at the intersection of East Butler Avenue and Shady Retreat Road, improvements depicted include sidewalks with a landscaped strip, pedestrian-scaled period lighting, street trees, and on-street parking. Also depicted is an element of access management, whereby the driveway access onto Butler Avenue has been eliminated. Much like the previous cross-section illustration, this graphic is meant to show how the design solutions could be applied within the segment. Site specific design solutions would be decided upon as development and redevelopment occurs.

Figures 20. Before/After Photosimulation of Butler Avenue looking west at Shady Retreat



**BEFORE** – Looking west on Butler Avenue at intersection with Shady Retreat Road



**AFTER** – Looking west on Butler Avenue at intersection with Shady Retreat Road: Displaying on-street parking, period style lighting, landscape strip, and street trees

## GATEWAYS

A gateway is a physical feature or geometric landmark that indicates a change in environment and demarcates limits with hopes of lower vehicular speed for safety and retail purposes. They are commonly a combination of street narrowing, medians, signing, archways, planting strips, or other identifiable features signifying a place.

### WESTERN ENTRANCE GATEWAY – BRISTOL ROAD & WEST BUTLER T INTERSECTION

This gateway area signifies the entrance to New Britain Borough’s ‘main street’ corridor traveling east from Chalfont Borough on West Butler Avenue. SEPTA railroad tracks splice through this juncture and railroad crossing gates control vehicular traffic when the passenger train is running. Any proposed sidewalks or trails should connect with already established sidewalks and trails that Chalfont Borough has constructed or planned. The gateway can be located at the northeast corner by the 530 Bristol Road Office Complex as shown in Figure 21 below. Figures 22 and 23 show the plan view and cross-section views of this gateway.

#### DESIGN SOLUTIONS

- Materials to complement the Eastern Gateway (University Village); incorporate identified common elements for uniformity
- Forms will complement existing ones: arches and horizontal wall spans as proposed for the Eastern Gateway (University Village)
- Consider traffic control measures (signage and signalization)
- ADA compliant curbs and crosswalks
- Low growing ornamental landscaping to complement existing plantings

**Figure 21. Western entrance gateway Bristol Road and West Butler Avenue**



Figure 22. Western Gateway Concept Plan

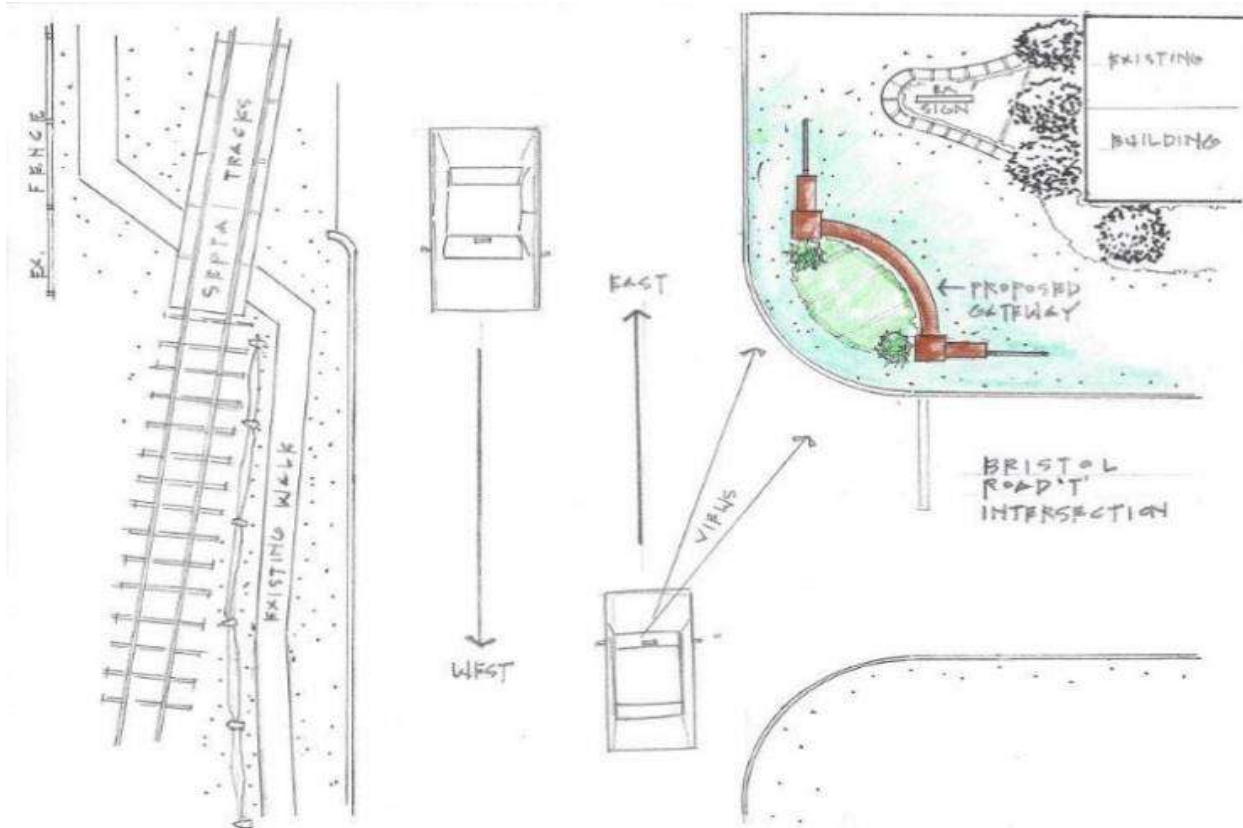
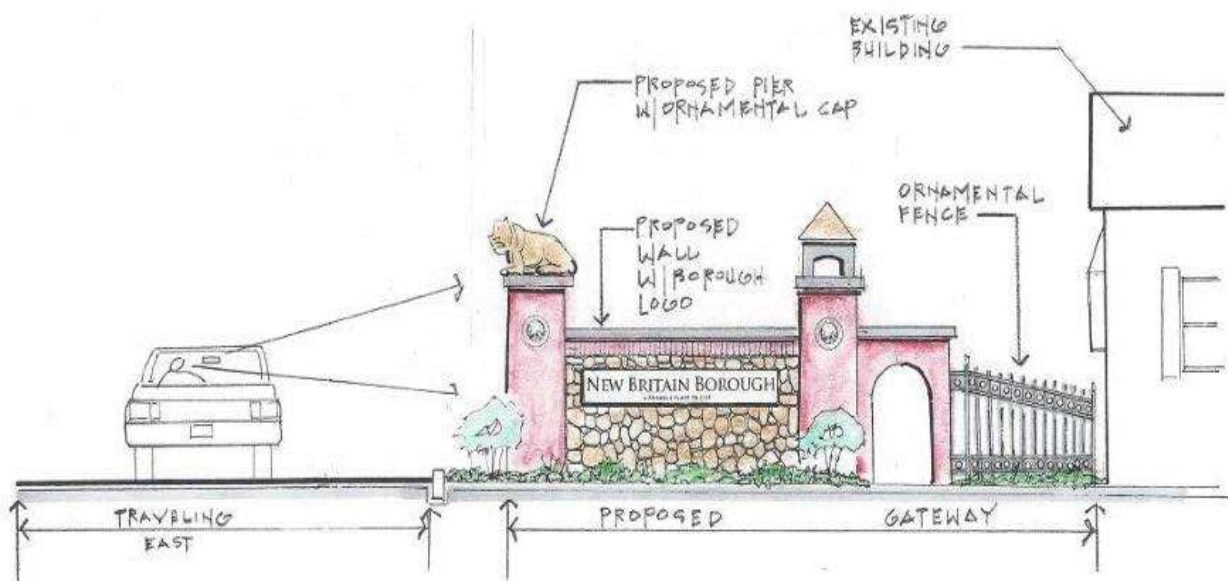


Figure 23. Western Gateway Concept Cross-Section Traveling East on West Butler Avenue



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## WESTERN GATEWAY (UNIVERSITY VILLAGE) – BEULAH ROAD & CONVERGENCE OF EAST BUTLER AVENUE INTERSECTION

This gateway can be sited at the southeastern corner in the lawn area adjacent to the Byrne Sewing Connection parking lot as shown in Figure 24 below. No crossings are proposed since it is not a signalized intersection. This gateway will define the outer edge of the ‘opportunity area’ and welcome travelers into the University Village as they travel east. The subsequent illustrations represent concepts only. Final design and placement will be guided by field measurements to insure safe sight distance.

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### DESIGN SOLUTIONS

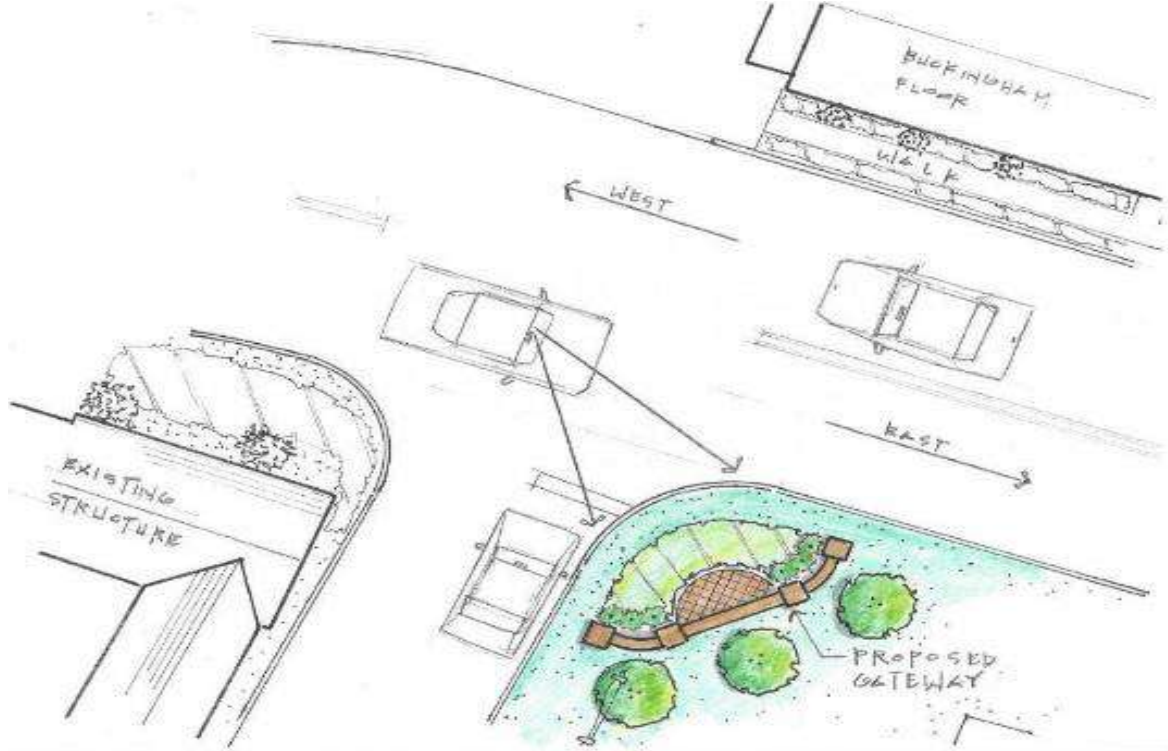
- Elements added will signify and complement themes associated with the University Village area
- Elements to be similar to the Eastern Gateway (University Village)
- Materials to be determined; incorporate identified common elements for uniformity
- Low growing ornamental landscaping to complement existing plantings

**Figure 24. University Village Western Gateway—Existing Conditions**



Figure 25 shows the University Village western gateway concept in plain view and demonstrates how the gateway would alert motorists traveling east along Butler Avenue to a change in the segment's composition. Figure 26 and Figure 27 provide cross-section views of the gateway concept.

**Figure 25. University Village Western Gateway Concept Plan**



**Figure 26. University Village Western Gateway Concept Cross-section Traveling East on East Butler Avenue**

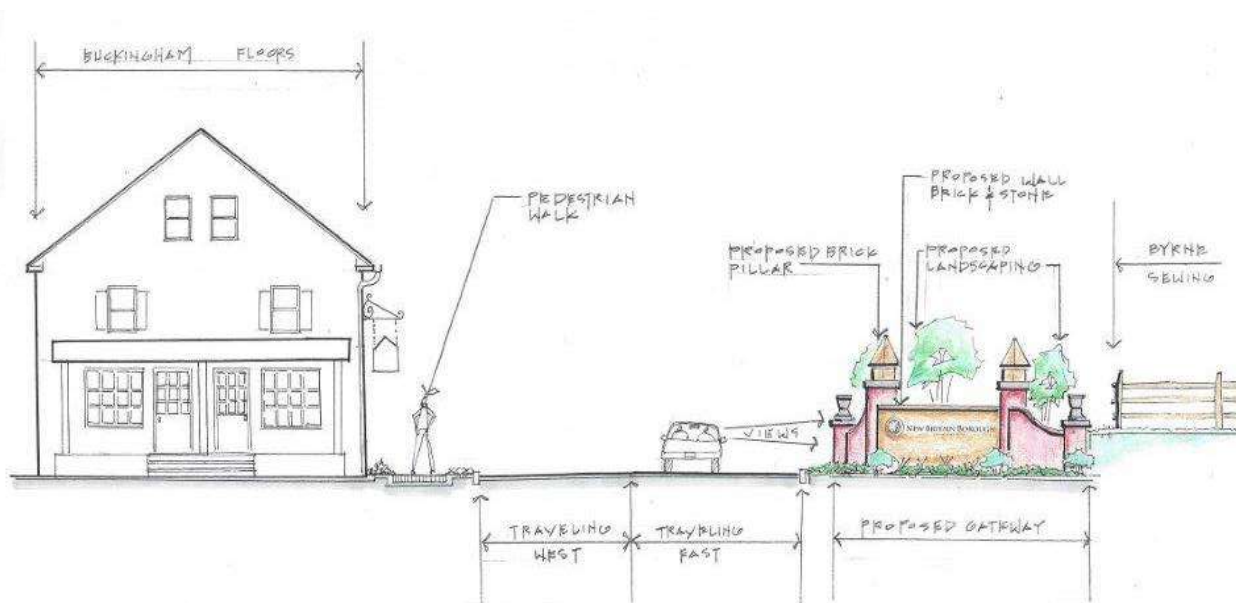
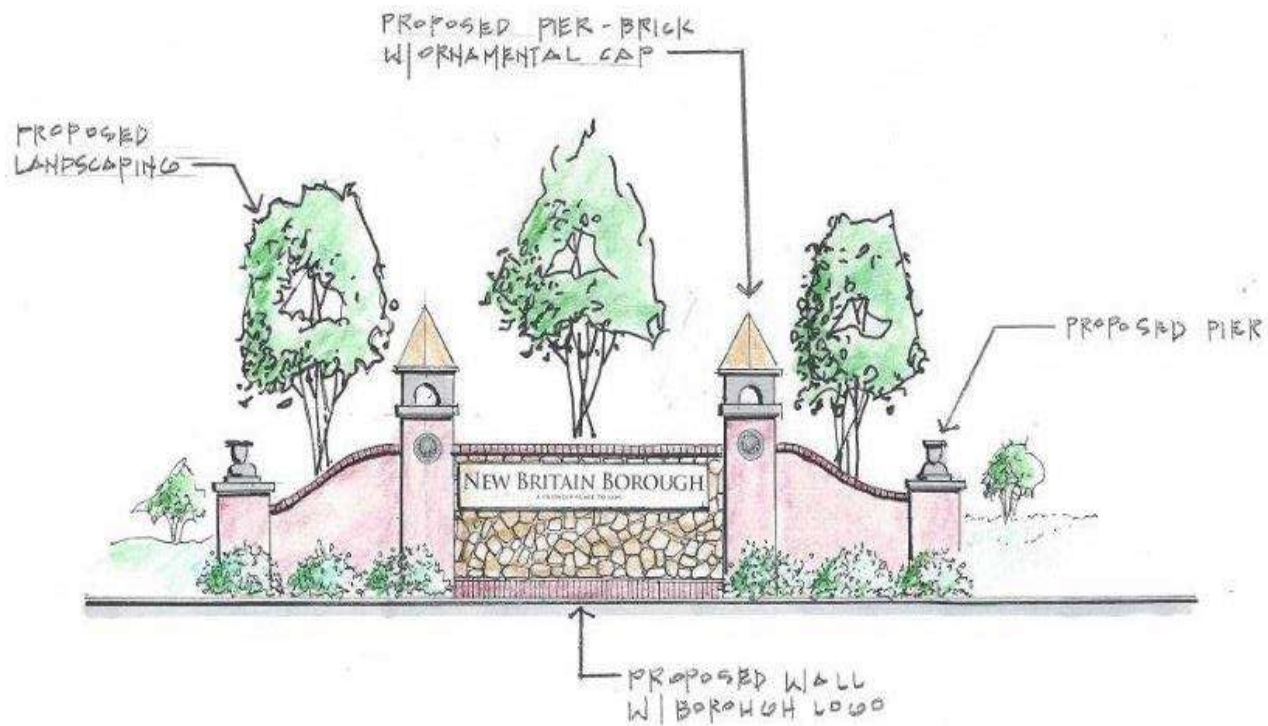


Figure 27. Elevation of Gateway Concept



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### EASTERN GATEWAY (UNIVERSITY VILLAGE) – NEW BRITAIN ROAD & CONVERGENCE OF EAST BUTLER AVENUE/STATE STREET INTERSECTION

This gateway area signifies the border and beginning of the Delaware Valley University frontage along East Butler Avenue as well as announcing entrance into the ‘main street’ corridor. It contains recently improved crosswalks, a landscape refuge median and ornate brick pillars, walls, landscaping, and a decorative iron gate. The new Life Sciences Center building is prominently displayed in the viewshed of travel along the two intersecting streets.

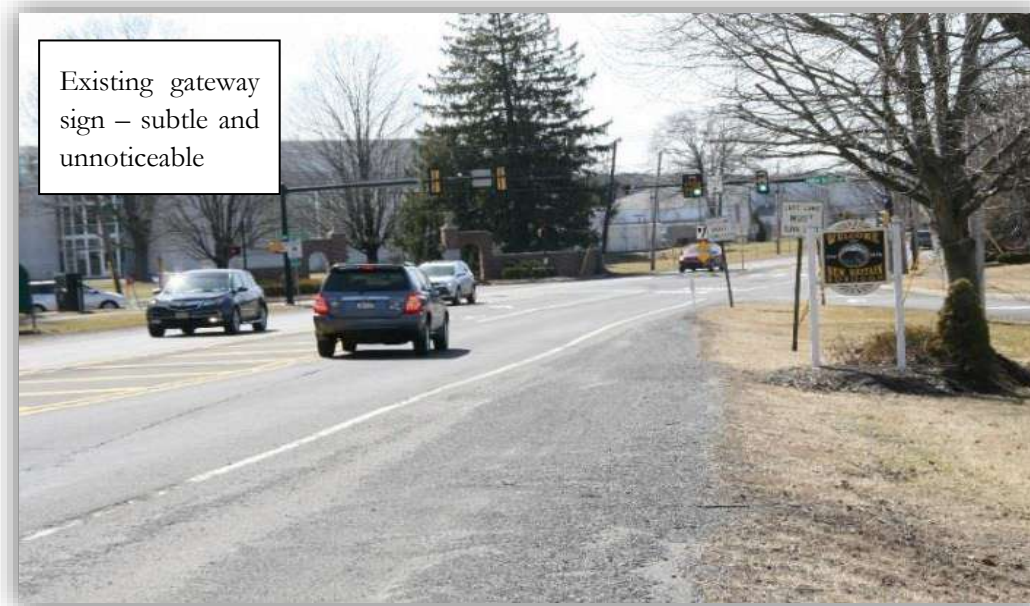
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#### DESIGN SOLUTIONS

- Additional elements added to this area will be on the opposite side of the Delaware Valley University gateway at the traffic signal as it will designate the entry into the Borough and the University Village segment
- Materials to complement the other two gateways; incorporate identified common elements for uniformity
- Materials to include brick, concrete coping
- Forms will complement existing ones and other gateways: arches and horizontal wall spans
- Low-growing ornamental landscaping

Figure 28 shows the existing gateway sign at East Butler Avenue and New Britain Road. The sign, while appealing, is subtle and unnoticeable because of its location and the other roadway features (signs, traffic signals, etc.) that draw attention from it. Figure 29 depicts a photosimulation from the *Butler Avenue Revitalization Strategies* (2009) study of an enhanced gateway entrance situated beyond the existing sign.

**Figure 28. Existing Borough Gateway Sign**



**Figure 29. *Butler Avenue Revitalization Strategies* Photosimulation**

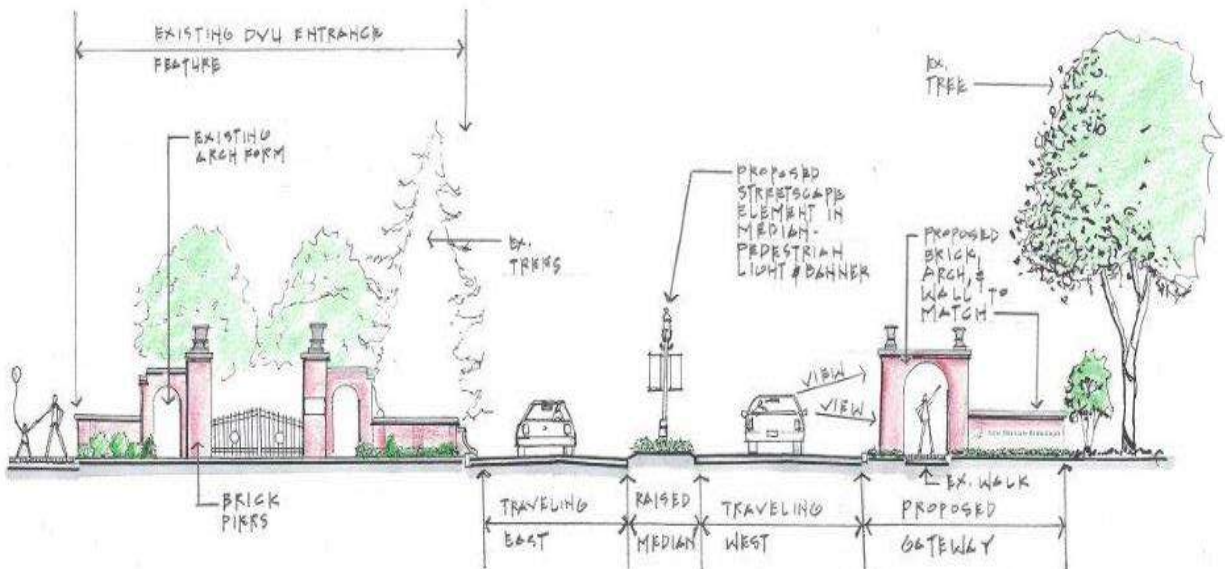


Figure 30 shows the existing conditions at the eastern entrance to the Borough at East Butler Avenue and New Britain Road. The Delaware Valley University entrance is at the left side of the picture. Figure 31 illustrates the proposed gateway concept with brick archway opposite the Delaware Valley University entrance.

**Figure 30. University Village Eastern Gateway—Existing Conditions**



**Figure 31. University Village Eastern Gateway Cross-Section**



## IDENTIFIABLE FEATURES

There are notable features along the study area that can be used in developing and designing the proposed streetscape elements. These elements include materials and forms that can help brand and identify the “Main Street” environment. Elements include:

- Brick in pillars and walls: Delaware Valley University entrance, American Heritage Federal Credit Union Building, various residence and building facades, H.S. Smiley, Inc., New Britain Inn
- Arch form: Delaware Valley University entrance, American Heritage Federal Credit Union Building, Children of America
- Light Fixture: American Heritage Federal Credit Union Building, Fluehr Funeral Home
- Perennial groupings: Alumni Lane entrance, Bucks-Mont Party Store
- Pennsylvania field stone building and wall: Canaan Cabinetry/Buckingham Floor and environs, New Britain Baptist Church, Lenape Valley Church, New Britain Veterinary Clinic
- Stone well house: Canaan Cabinetry/Buckingham Floor, New Britain Veterinary Clinic
- American Flag: Fluehr Funeral Home, Canaan Cabinetry
- Columns on porticos and porches: intermittent residences
- Lion statues: Painted Shutter Country Store, Lion House

The following pictures illustrate architectural features and forms found within the study area. They are intended to provide examples of the elements discussed above.



Statues



Flagpole



Arch Form & Brick



Period Light



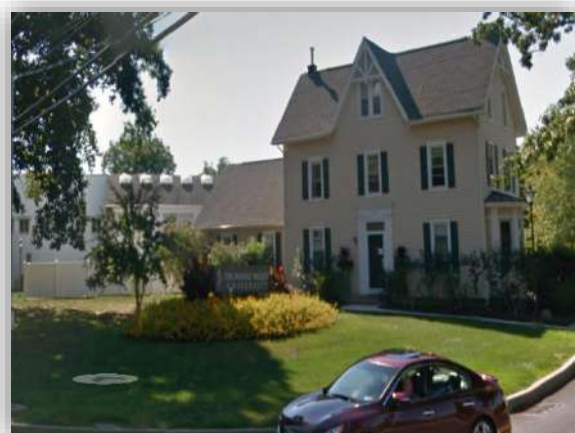
Stone Facade



Stone Facade



Arch Form & Brick



Ornamental Landscaping



Ornamental Landscaping



Well House & Stone Facade



Well House & Stone Facade

## DESIGN GUIDELINES

New Britain Borough has a rich inventory of buildings and architectural styles. The purpose of design guidelines is to define the qualities of architecture, building layout and site design, and public space that make for successful development, and to serve as a tool for guiding development that preserves, reflects, or takes inspiration from the architecture and physical environment that comprise the extraordinary and distinct character of the Borough. Although the design guidelines by themselves cannot guarantee good design, they are nonetheless a powerful element in creating development that contributes to an attractive, vibrant, and livable community, project by project.

The application of design guidelines is one of many planning tools that a municipality has at their disposal to help influence the type, nature, and appearance and character of development to meet their intended vision. Experience has shown that developers are often willing to work with a community, but they need input upfront from the municipality. However, often municipal officials are unclear about what they want their community to look like, which is the primary purpose of design guidelines. Design guidelines promote the ideal that buildings and properties are assets that should stand the test of time and are sources of community pride. They are intended to provide greater clarity and guidance for developers, landowners, and reviewing entities for a variety of design issues, which can include architectural and building composition, historic preservation, sustainability, site and streetscape development, public spaces and amenities, signage and light placement, walls, fencing and landscaping, and outdoor dining and sidewalk cafes. In order to be user-friendly and effective, design guidelines must contain detailed provisions along with photos, illustrations, and diagrams that are clear, concise, and implementable.

Appendix 5 contains model design guidelines in ordinance format that are intended to be used as a template that could be tailored to satisfy the Borough's unique vision. An option is to apply the design guidelines as part of an overlay district that can be placed in the appendix to Borough ordinances. The Borough would identify which area within the Borough would be included in the overlay district.

Design review is the evaluation of the appearance of proposed buildings and structures as part of the subdivision and land development approval process, with its goal being the improvement of the aesthetic quality of the built environment. The Borough already utilizes design review as part of the recently-adopted Mixed Use Overlay District. This study and these guidelines provide the foundation and support necessary for design controls.

Municipalities interested in improving community character may turn to incentive approaches. Voluntary, nonbinding design review meetings, early in the application process before too many design decisions have been made by the developer, may be held between the developer and municipal staff. Requiring compliance with design review to obtain bonuses through incentive zoning may be a way to implement design review.

## ECONOMIC DEVELOPMENT STRATEGIES

Economic development can improve a community's well-being through job creation, business growth, income growth, and enhanced quality of life. Successful economic development requires a multi-pronged approach that takes into account the local market and business environment, engages business owners and residents, builds on a community's assets, engages local community leaders, and is championed by local officials.

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### ECONOMIC DEVELOPMENT POLICY

The following policies form the economic development strategy for the corridor. These policies have been developed with the objective of implementing the vision set forth and strengthening the economic prosperity of the Borough, its businesses and residents.

The Borough has convened a Community and Business Committee comprised of 12 members, six of whom are residents and six of whom are business owners within the Borough. Borough staff and consultants, Borough Council members, and the Bucks County Planning Commission provide support for the committee. The Committee's goal is to create an environment that attracts targeted businesses, promotes current businesses, enhances the quality of life for residents, and improves the tax base. This Committee should continue to serve as the coordinating entity to engage property and business owners to understand their needs and convey the concepts and vision of the plan, develop and coordinate marketing efforts, secure funding for improvements, and ensure that the activities and recommendations of the plan continue to be carried out in accordance with the principles of the plan.

***Support the common Vision***—The Butler Avenue Corridor Revitalization Plan chapter provides a vision statement that states the desired outcome and conditions of the corridor in the future. The Borough's efforts in economic development should support this vision.

***Support the Community and Business committee and Borough Council's efforts***—This support should continue as the Community and Business Committee continues the following activities:

- Act as a champion for the corridor and the plan;
- Engage property and business owners to understand their needs and to further develop partnerships;
- Promote the concepts and vision of the plan;
- Develop and coordinate marketing efforts;
- Secure funding for improvements;
- Ensure the activities and recommendations of the plan continue and are carried out in accordance with the plan.

***Market the corridor***—Marketing of Butler Avenue as a destination and a place to do business should continue as a primary focus of the Community and Business Committee. The Committee, under Borough Council’s supervision, has invited business owners to participate in the Committee work and sought their opinion and input. The Committee has featured new and existing businesses in its newsletter. Future efforts to brand and market the area and to encourage residents to “Shop Local” are important components for a successful Main Street.

***Seek out new business***—The Market Analysis concluded that the following businesses represent potential opportunities for new business within the corridor:

- Full-Service Restaurants
- Limited Service Eating Places
- Special Food Services
- Drinking Places –Alcoholic Beverages
- Furniture and home furnishing stores
- Clothing and Accessory Stores
- Sporting goods, hobby, and musical instrument stores
- Florists
- Office supplies, stationery, and gift stores
- Other miscellaneous store retailers

The Mixed Use Overlay ordinance permits these uses and requires design review to ensure that the vision for the corridor is carried out. The Borough’s underlying zoning districts also allow these uses in appropriate areas, with the land development process allowing for scrutiny by Borough officials to ensure that the vision is respected.

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## POTENTIAL FUNDING SOURCES AND TAX INCENTIVES

Aside from private investment in the corridor, there are potential funding sources and tax incentives that the Borough could consider to spur economic development. These funding sources are in addition to those discussed in the Transportation and Mobility chapter of this plan. Appendix 3 describes potential funding sources, as well as the requirements of each program.

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### *LOCAL ECONOMIC REVITALIZATION TAX INCENTIVE LAW (LERTA)*

The LERTA law allows businesses to make improvements or to expand their businesses but defers for a period of years any property tax increases that would result from a reassessment of their improved property. To be eligible for this tax abatement program, properties must be located in a “deteriorating area,” as determined by the borough council. By phasing in any real estate tax increases that would result from property improvements, business owners have an incentive to repair, alter, and add to businesses.

The three taxing bodies in New Britain Borough – the county, school district, and the borough – may each vote to allow for the phasing in of real estate taxes over a period not exceeding 10 years. For the taxing bodies, there is no loss of current tax revenue because taxes are not reduced. The only impact is that the incremental increase that would occur due to the property improvement is deferred in accordance with a time frame that the borough selects.

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#### *BUSINESS IMPROVEMENT DISTRICT*

A **business improvement district** is a defined area within which businesses pay an additional fee in order to fund projects within the district's boundaries. The assessments collected are used for the purpose of delivering supplemental public services to the districts. One example in Pennsylvania is the West Chester business improvement district formed to fund and implement programs that increase customer traffic and sales, retain, expand, and recruit viable businesses; and advocate the interests of the downtown to government agencies.

## LAND USE PLAN

The Land Use Plan brings together the recommendations and strategies from the Mobility, Design for Main Street, and Economic Development sections of the plan to provide a series of segment plans for the Town Center, Historic Village, and University Village areas that together represent the overall vision for the corridor. A vision statement, which is a description of the opportunity areas and proposed improvements that are unique to that area, is included for each of these segments. Opportunity Areas are places where redevelopment and development are likely to take place. The segment plans provide a visual representation of the vision for each area.

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### TOWN CENTER LAND USE PLAN

#### ***Town Center Vision Statement***

*The Town Center area serves as the Borough's western gateway. The commercial uses along Butler Avenue share a unified streetscape, are connected by a sidewalk system, and parking areas are located behind the buildings. The Town Center of New Britain Shopping center is a vibrant and aesthetically pleasing commercial area. Circulation within the site is both safe and efficient. Passive recreation amenities in the open space at the southwest corner of the site provide space for public events and for the enjoyment of pedestrians.*

The Town Center area comprises the corridor from the Borough's boundary with Chalfont at Bristol Road and continues to Lenape Drive. Within the Town Center area, there are a mix of commercial and residential land uses from Bristol Road to Ute Road and between Ute Road and Lenape Drive on the north side of the corridor is the Town Center of New Britain shopping center. Map 7 shows the Town Center segment plan.

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#### **OPPORTUNITY AREA**

The Town Center shopping center is the largest concentration of commercial use within the Borough. It comprises 15 acres and contains 124,000 square footage of commercial space. The anchor tenant is the Giant supermarket. Outparcels include a branch bank, a McDonald's restaurant, and Fusion Gym fitness center. At the southwest corner of the site is an approximately 38,000 square-foot lawn area. This area is used for Borough special events including an annual tree lighting ceremony. The site's layout is that of a typical community center with an expanse of parking in front of the commercial buildings which are set far from the road. There are few landscaped islands within the parking areas and circulation within the shopping center, both for vehicles and pedestrians, is poorly defined. The area along the frontage of West Butler Avenue has extensive open lawn areas that are used only occasionally for community events. Based upon observations of vacant parking spaces during the week and weekends, during peak and off-peak hours, there appears to be a surplus of parking onsite. Figure 32 shows the existing configuration of the Town Center shopping center.

**Figure 32. Town Center of New Britain Existing Conditions**



To capitalize on this underutilized site, two separate concept plans have been developed for the shopping center. The concept plans incorporate the recommendations from the streetscape chapter which include streetscape improvements, landscaping improvements, and further development of the lawn area at the front of the center. Streetscape improvements along the Butler Avenue frontage of the shopping center would include the completion of the sidewalk and pedestrian-scaled street lights with arms for banners. The proposed expansions would mean the elimination of some parking areas, but a parking analysis shows that the remaining parking spaces should suffice in serving the site if an adjustment to the parking ratio is provided (i.e., reduction of the minimum parking ratio from 5.5 to 4.0 parking spaces per 1,000 square feet of floor area). This slightly lower parking ratio appears more realistic and in line with current parking standards used for shopping centers throughout the County.

In the Public Space Option (see Figure 33), further development of the lawn area in the southwestern corner of the shopping center as a public green space would allow for greater use of this largely underutilized space for public events and passive recreation. The addition of sidewalks and paths, benches, and trash receptacles and a fountain or plaza, would also promote its use as a passive recreation park. This concept plan incorporates grass pavers in the public green space that would allow for intense use of this area to accommodate community fairs and festivals, car shows, and food trucks.

**Figure 33. Town Center of New Britain Public Space Option**



The Retail Option (see Figure 35) is intended to accommodate additional retail space in lieu of the public green space. Barring any obstacles to this addition of retail space, there may be significant benefits to the Giant supermarket and the shopping center as a whole. A synergy may be formed between the new and existing retail areas by attracting customers that patronize the various establishments within the Town Center complex. The new retail uses could include shops and restaurants with outdoor public spaces, plazas, and possibly outdoor dining, which collectively would increase the viability and attractiveness of the Center from residents living within the Borough and within the region.

**Figure 34. Town Center of New Britain Retail Option**



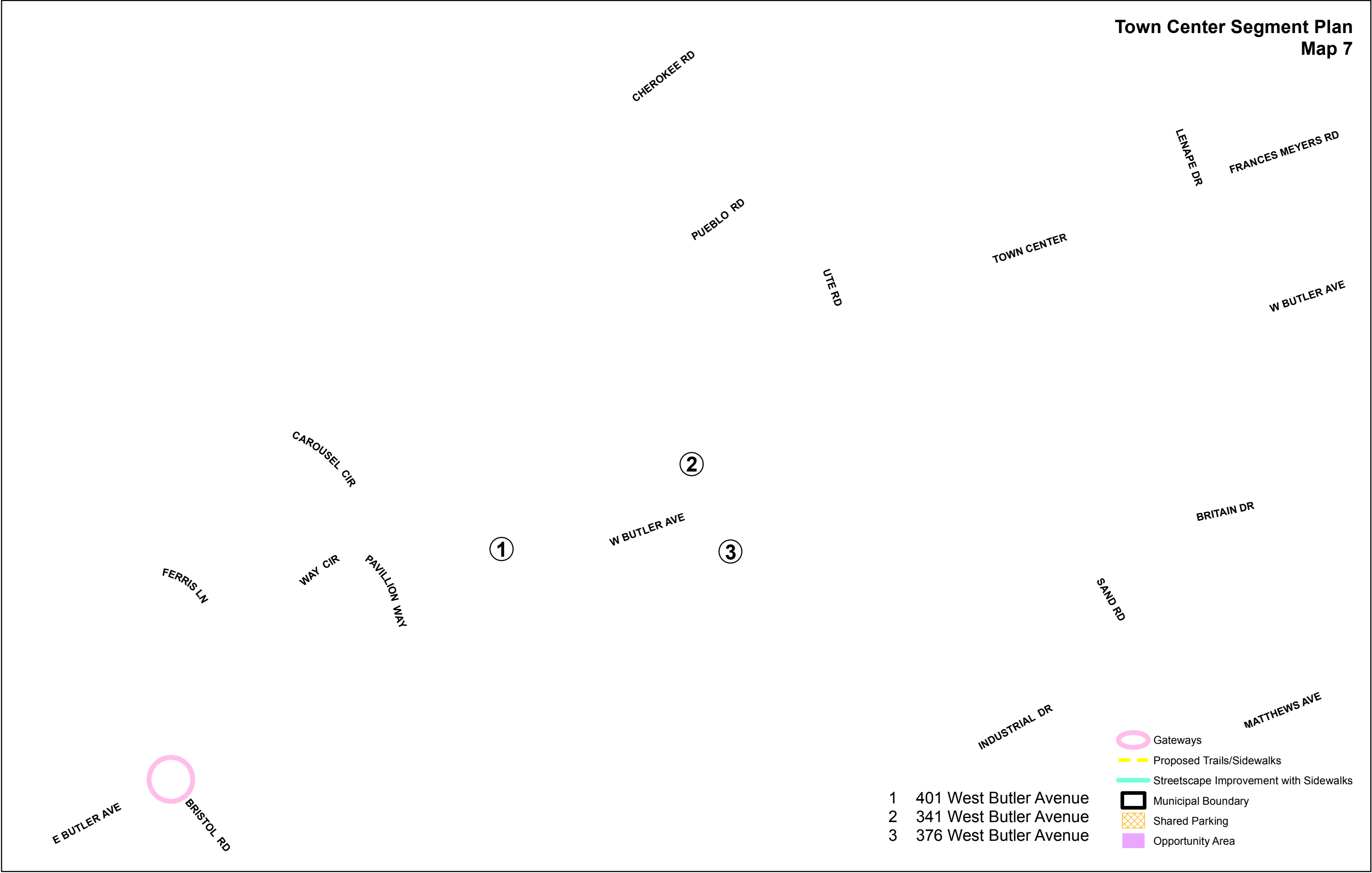
In both the Public Space and Retail Option concept plans, there are additional landscape islands at the end of parking aisles and parking lot plantings where they are currently missing. Landscape islands improve aesthetics, reduce impervious surface, provide shade for customers, and define travel lanes to enhance vehicle circulation onsite. The other benefit of both options is improved traffic circulation across the entire shopping center connecting Lenape Drive to Ute Road. Both concept plans provide an expansion of the lawn area adjacent to the bank with additional crosswalks and sidewalks to connect to the southwest public green space/retail area. In both concept plans, two new pedestrian spines with landscaped walkways are intended to create an upscale appearance, linking the new retail area or public spaces and, the existing retail areas with landscaped walkways. These walkways together with new sidewalks along both sides of West Butler Avenue will enhance pedestrian access throughout the Town Center area. Smaller tree species with an upright branching habit can be planted in the public spaces/lawn areas and landscape islands, so that view to the adjacent buildings are not obscured but the overall aesthetics on the site is greatly improved.

As of October 2016, there were three vacancies within the centers 24 leasable spaces. Vacancies within shopping centers can contribute to a negative view of both the shopping center and the community. The Borough should continue its dialogue with the shopping center owners to assess problems and identify ways to be competitive.

The marketing of vacant spaces based upon an analysis of regional leakages and surpluses provided in the Economic Development section may be useful to the success of the shopping center. The food and drink industry was identified as a possible opportunity. Establishments within this category included full-service restaurants, limited-service eating places, special food services, and drinking places – Alcoholic Beverages. Retail opportunities were identified in furniture and home furnishing stores; clothing and accessory stores including clothing, shoe, jewelry, luggage, and leather goods stores; sporting goods, hobby, and musical instruments stores; florists, office supplies; and stationery and gift stores.

The existing land use survey identified three properties within the Town Center area that were considered underutilized. The segment plan also depicts these as opportunity areas. These sites include 341 West Butler Avenue and 401 West Butler Avenue which are located on the north side of West Butler Avenue and 376 West Butler Avenue on the south side. Each of these sites have the potential to be further developed for additional commercial development. If these sites are subdivided for commercial use or redeveloped, the planning concepts discussed here, such as access management, shared parking, streetscape improvements, and design review should be considered during the development approval process.

Town Center Segment Plan  
Map 7



## STREETSCAPE AND GATEWAY

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Streetscape improvements within the Town Center area will serve to connect the shopping center and the other commercial and institutional uses within the segment as well as designate the area as a major entrance to the Borough. Essential improvements include the completion of the sidewalk network along both sides of Butler Avenue, pedestrian-scaled street lights with arms for banners, and a gateway feature.

A gateway can be located at the northeast corner by the 530 Bristol Road Office Complex (TMP #25-1-6). An alternative location would be near the existing Borough sign at 470 West Butler Avenue (TMP #25-1-8). The final location of the gateway feature will be determined based upon further study and communication with property owners. The Design for Main Street section provides more specific details on the recommended streetscape improvement and gateway feature.

## PEDESTRIAN AND BICYCLE MOBILITY

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The segment plan recommends providing sidewalks along the north side of West Butler Avenue from Bristol Road to Lenape Road and on the Pine Run Elementary School site from Butler Avenue to the building's main entrance. Crosswalks are needed at Bristol Road, Ute Road, and Lenape Road. The current crosswalks at Sand Road should be upgraded to meet the proposed streetscape preferred design solution. On the south side of Butler Avenue sidewalks are proposed from Bristol Road to Lenape Road ending at the gas station at 194 West Butler Avenue (TMP #25-2-29-1).

The improvement of the sidewalk on Sand Road will be an important connection with the shopping center and to the proposed trail that follows the rail line. Employees of the industrial establishments along Britain Road would benefit from easier and safer passage to the shopping center.

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## HISTORIC VILLAGE LAND USE PLAN

### ***Historic Village Vision Statement***

*The Historic Village area has continued to endure as the historic center of the Borough. The historic residences, commercial buildings, and places of worship have been maintained as a reminder of the Borough's past. The Historic Village area is connected to the Borough's commercial areas at the western and eastern ends of Butler Avenue by a unified streetscape that includes pedestrian-scale lighting, sidewalks/trails, and wayfinding signs.*

The Historic Village area is the section of the corridor from Lenape Drive to Beulah Road. This area encompasses the heart of the Borough's historic village at the intersection of Tamanend Avenue and Butler Avenue as well as established residential areas. There are some nonresidential land uses within this area but it is largely residential. Butler Avenue curves slightly as it travels through this part of the Borough. This, combined with the larger lot residential uses, gives this part of the Borough a more

open and airy character as compared to the other two more densely developed segments. Map 8 shows the Historic Village segment plan.

The segment plan does not depict or propose changes to the land use patterns established for this area of the Borough. Instead, the goal is to preserve the land use patterns, historic residences, and other features that contribute to the historic nature of the area. Connections to the other segments of the corridor will be important.

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### *STREETSCAPE IMPROVEMENTS*

Streetscape improvements within the Historic Village area will serve to unify the area with the more commercial areas of the Town Center and University Village areas located to the west and east respectively. The sidewalk or macadam trail on the north side of Butler Avenue would be lined with pedestrian-scale period lighting with banners. Also important will be the wayfinding signs, as shown on Map 8, at the entrances to the historic area along Butler Avenue and Tamanend Avenue. These signs would point the way to the important destinations within the area including the Borough offices, the New Britain Baptist Church, New Britain SEPTA Station, and the Town Center and University Village areas. These signs would also serve as gateway features to identify the area.

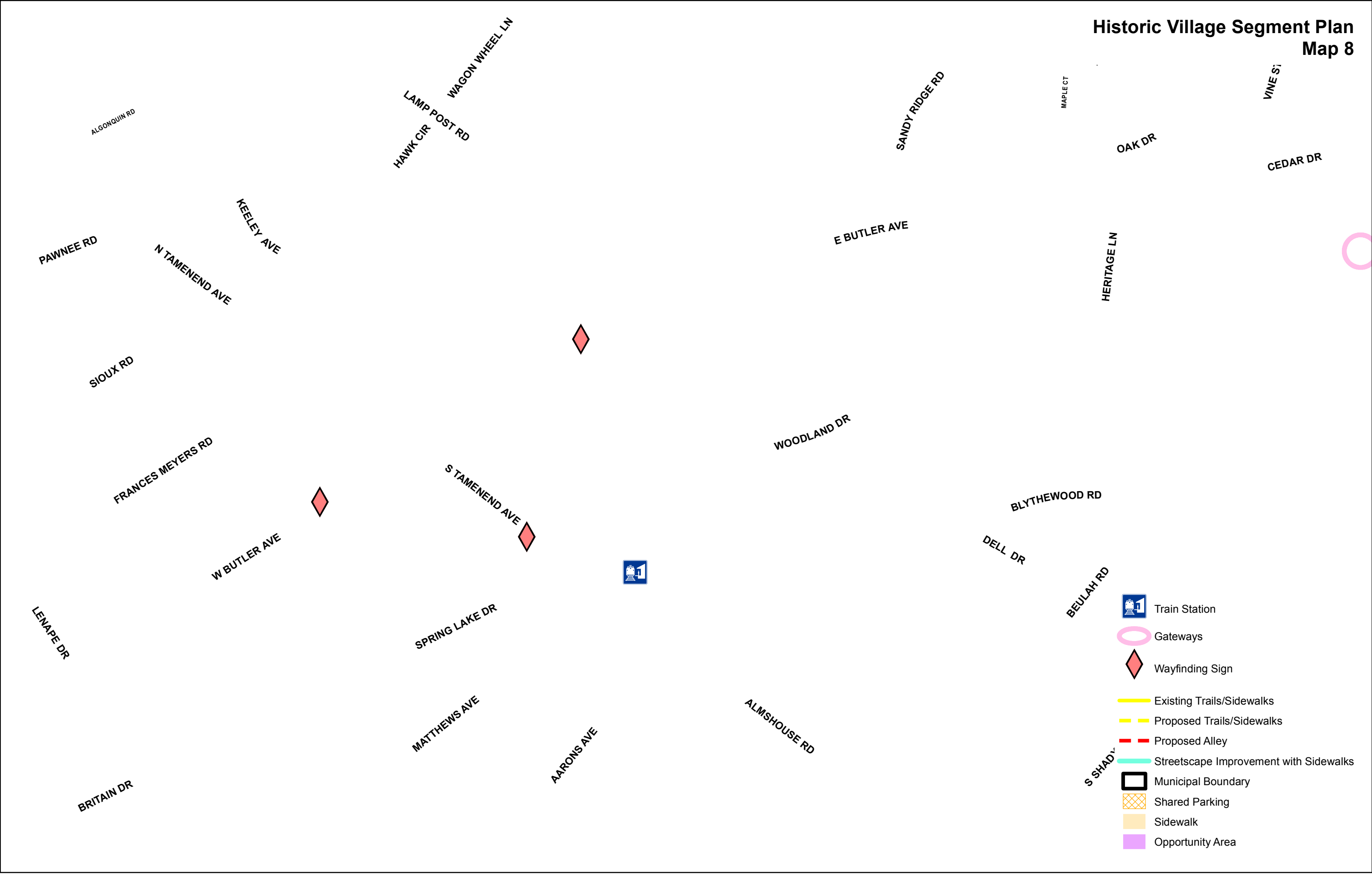
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### *PEDESTRIAN MOBILITY*

The primary feature of the concept plan for the Historic Village is the proposed sidewalk or macadam trail that would provide the pedestrian connection to both ends of the Borough. The plan shows the sidewalk/trail being placed on the north side of Butler Avenue for much of its path. Construction of this path will not be without some difficulty because of the limited right-of-way width and the residences and stone walls located in close proximity, sometimes within 5 feet, to Butler Avenue near the Tamanend Avenue intersection. Careful site analysis and design is needed to preserve these buildings and walls. An alternative, but less desirable, route for the path would follow Lamp Post Road to Keeley Avenue, then to Sioux Road, and onto Tamanend Road where it would connect back to Butler Avenue.

At the western end of the area between Lenape Drive and Tamanend Avenue are two service roads, Access A (the western road) and Access B (the eastern road), that provide access to the residences on the north side of West Butler Avenue. Sidewalks exist along much of these two roads except along four properties. A sidewalk segment between the two roads is proposed. The service roads are separated from Butler Avenue by two landscaped islands that are owned and maintained by the Borough. Street trees have been planted within these islands which provide a buffer for the residents and pedestrians. The trees on the Access B island appear healthy and do not interfere with the overhead utility lines. However, many of the trees on the Access A island have grown into the utility wires. A replanting of appropriately sized street trees is required. Benches could also be added to these areas for pedestrian convenience.

Historic Village Segment Plan  
Map 8



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## UNIVERSITY VILLAGE LAND USE PLAN

### *University Village Vision Statement*

*The University Village area is a vibrant commercial and residential district with a distinct “Main Street” character and a sense of place. The business community in this area serves Borough residents, visitors, and the students and faculty of Delaware Valley University. The University Village area is connected to the Historic Village area, and ultimately to the Town Center area by a unified streetscape that includes pedestrian scaled lighting, sidewalks/trails, and wayfinding signs. Gateway features at either end of the University Village signify to motorists and pedestrians the arrival at a unique place.*

The University Village area is the eastern most segment of the corridor and is from Beulah Road to the Borough boundary near New Britain Road. The land uses within this area are predominately nonresidential with a mix of commercial and office uses. The campus of Delaware Valley University is the largest single land use within the segment. Map 9 shows the University Village segment plan.

There exists a symbiotic relationship between the University and the Borough that, through the realization of the vision for a University Village, can be heightened. The University will benefit from the creation of a vibrant University Village that serves the campus community and that is seen as an attractive amenity to potential students. The Borough will benefit, both economically and visually, by the revitalization of its Main Street corridor.

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### OPPORTUNITY AREA

The entire University Village area is considered an Opportunity Area for the purposes of this plan. There are several reasons for the designation of such a large area as an Opportunity Area. Chiefly among them is that this part of the corridor is adjacent to the Delaware Valley University Campus, and the creation of a complementary commercial and residential district, a University Village, is one of the motivations for this study. Much of the area is in commercial use, and with the adoption of the Mixed Use Overlay district, the appropriate zoning for the desired type of development and redevelopment is in place.

A diverse mix of retail, commercial, personal service, office, institutional, and residential uses within the University Village is desired and necessary to create the vibrant neighborhood envisioned. These uses will provide for everyday needs, offer employment opportunities, and create recreation and civic opportunities for university students, faculty, and staff as well as Borough residents and visitors.

The existing land use survey identified six properties within the University Village area that were considered either underutilized or potentially available for redevelopment if the property owners choose to relocate or outgrow their current space. All sites are privately owned, so any development, new development, or changes would have to be initiated and supported by the property owners. These sites include the former Knoell property (TMP #25-9-2), the former Bitzer’s Cleaners (542 East Butler

Avenue, TMP #25-9-38), the former Duck Deli (524 East Butler Avenue, TMP #25-9-37), Smith Marine/Alley-Gator Shop (573 East Butler Avenue, TMP#s 25-9-19 and -20), Delaware Valley Concrete (20 South Shady Retreat Road, TMP #25-8-3-1), and Delaware Valley University Campus at the East Butler Avenue and Shady Retreat Road intersection (portion of TMP #25-9-90). These sites are located in close proximity to each other, forming a core redevelopment area within the University Village area.

Because of the imminent redevelopment of the vacant Knoell site, the Borough has an opportunity to guide redevelopment and be a catalyst for renewal in the University Village. The segment plan incorporates the initial site plan that the Knoell property developer has submitted to the Borough. The proposal includes 117 dwelling units and 21,000 square feet of commercial use in three 3-story buildings, two of which would be mixed use (commercial and residential) and the third would be residential only. The site plan was developed under the provisions of the Mixed Use Overlay District and includes many of the streetscape elements proposed for the corridor. Wide sidewalks along East Butler Avenue with a public open space between the buildings at the front of the site will encourage pedestrian activity. Parallel parking along East Butler Avenue and shared parking in the interior of the site, behind buildings, fulfill the proposed parking recommendations. Connections to the proposed trail system along North Shady Retreat Road have also been incorporated.

Figure 35 shows the existing conditions (Before) looking east at the intersection of East Butler Avenue and Shady Retreat Road. Figure 36 is a photosimulation (After) that incorporates the recommended streetscape elements and would meet the vision and design guidelines for this segment. Notable improvements to the streetscape at this site would include parallel parking, wide sidewalks, period-style street lights, public spaces, and a build-to line that requires buildings to be placed close to the street. As required by Borough subdivision and land development ordinance, the overhead utility lines would be buried for all redevelopment projects on the site.

The photosimulation includes improvements on both sides of East Butler Avenue and shows in general what development on the Knoell site could look like. The Promenade at Sycamore development along Sycamore Street in Newtown Township was chosen as a representative design for the development and has been used as the example. While this does not represent the ultimate design for the Knoell site, this graphic provides a visual representation of what the corridor could become and can be used as a starting point for future development and redevelopment projects. Final design of the Knoell site will evolve through the land development process with input from the various stakeholders including the developer, Borough Planning Commission, and Borough Council.

**Figure 35. East Butler Avenue at Shady Retreat Road Looking East**



**Figure 36. Photosimulation of University Village redevelopment**



Adjacent to the Knoell site, along the southern side of East Butler Avenue and Shady Retreat Road, is a portion of the Delaware Valley University Campus that, due to its proximity to the Knoell property, has development potential. This property is located in Doylestown Township, and development there will be guided by their Township codes and policies. The Borough has begun a dialogue with the Township to discuss shared planning issues including development and redevelopment along Butler Avenue and maximizing the potential provided by the two SEPTA stations located in Doylestown Township that are used by the University and by the residents of New Britain Borough. Future development in the corridor can take advantage of transit access by using a transit-oriented development approach with proper land use regulations in place that would encourage a walkable, pedestrian-oriented environment that reinforces the use of public transportation.

The segment plan depicts development on the Delaware Valley University campus property that would mirror what is proposed on the Knoell site (see Map 9). Shared parking, an alley system, and sidewalks/trails would connect this site to the former Bitzer's Cleaners (542 East Butler Avenue, TMP #25-9-38) and Duck Deli (524 East Butler Avenue, TMP #25-9-37) sites and to the row of properties on the south side of East Butler Avenue from Shady Retreat Road to Iron Hill Road/Alumni Lane.

The segment plan also depicts potential redevelopment of the Smith Marine/Alley-Gator Shop (573 East Butler Avenue, TMP#s 25-9-19 and -20), located on the north side of East Butler Avenue at its intersection with Barrie Circle, to a layout that would be consistent with the University Village concept (See Map 9). The proposed buildings are similar in size and maintain consistent setbacks with those shown on the Knoell site. Driveways along Barrie Circle for the Knoell site and this site should be aligned to reduce conflict points between vehicles and promote the interconnectedness of the area.

The Delaware Valley Concrete site is located behind the row of commercial properties that front the south side of Butler Avenue between Beulah Road and South Shady Retreat Road and does not have direct access to Butler Avenue. Potential redevelopment of this site to a more appropriate use for the University Village would benefit the corridor. The segment plan depicts shared parking areas and an alley between Beulah Road and Shady Retreat Road. These improvements could take place with or without the redevelopment of the Delaware Valley Concrete site, but together would create an interconnected development. Proposed buildings are not shown on the segment plan but should be situated with respect to the existing land uses and to the potential development on the Delaware Valley University site across South Shady Retreat Road.

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### *STREETSCAPE AND GATEWAYS*

Streetscape improvements within the University Village will be an especially important element of the entire corridor's revitalization. This segment is the most densely developed part of the corridor; therefore, the full range of the streetscape elements are proposed within this area. The Design for Main Street section provides more specific details on the recommended streetscape improvements for the University Village area. The segment plan recommends sidewalks along both sides of East Butler Avenue and crosswalks at intersections throughout the University Village area. Connections to the

regional trail system would be incorporated at North and South Shady Retreat Roads and Iron Hill Road.

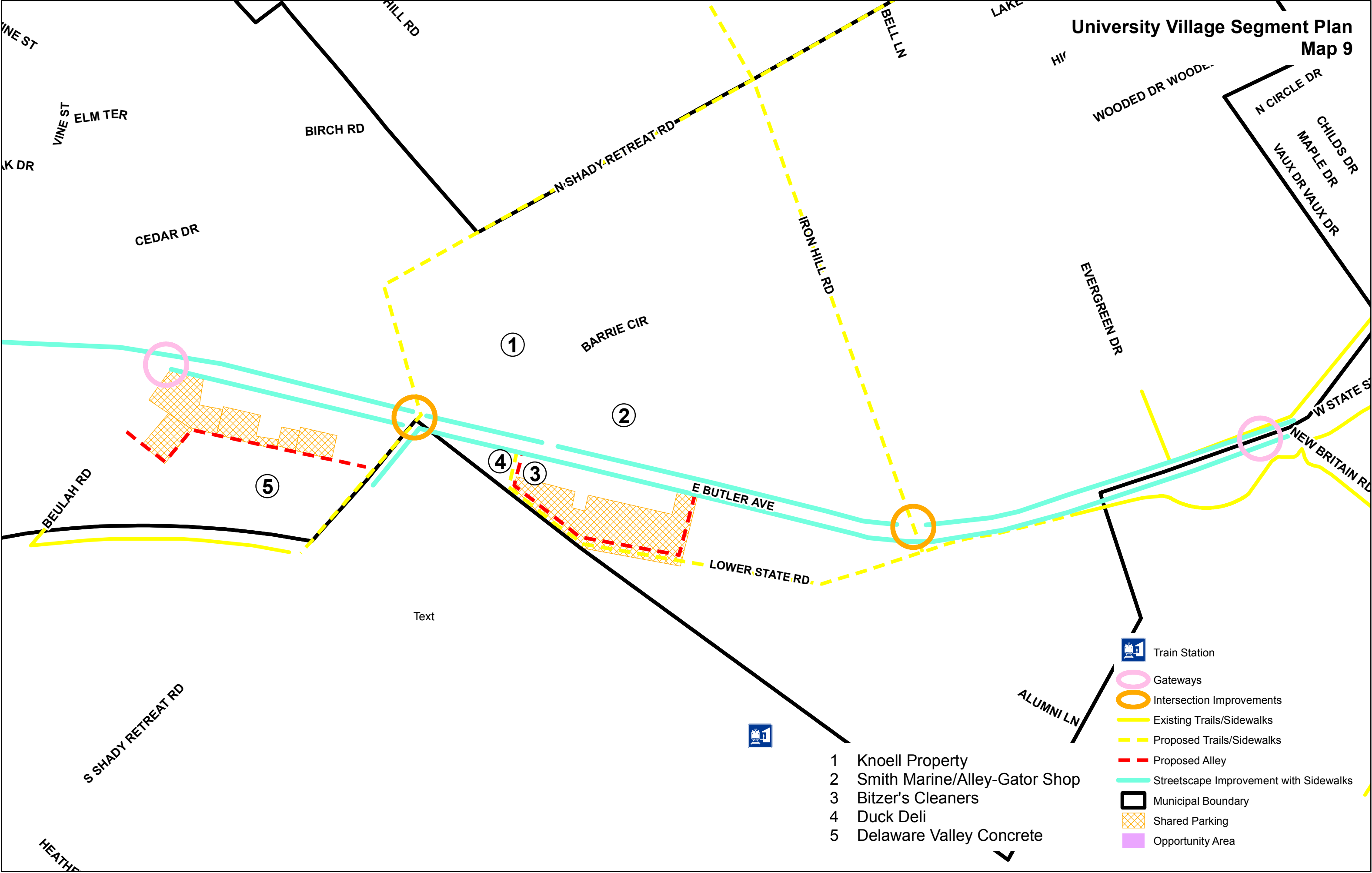
Two gateway features, a western and eastern gateway, at Beulah Road and New Britain Road, respectively are proposed. These gateway features are particularly important given the high amount of traffic and vehicular speeds in this part of the corridor. The streetscape section shows recommended locations for these features but the final location of each gateway feature will be determined based upon further detailed study and communication with property owners.

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#### *INTERSECTION IMPROVEMENTS*

The segment plan depicts two intersections designated for improvement. The Mobility section proposes a roundabout at the intersection of East Butler Avenue and Shady Retreat to resolve the poor alignment and multiple conflict points of the intersection. Timing of this extensive improvement should be coordinated during the land development process for the redevelopment of the Knoell site. The second intersection improvement shown corresponds to the East Butler Avenue and Iron Hill Road/Alumni Land intersection at the main entrance to Delaware Valley University. Possible improvements, as recommended in the Design for Main Street section, could include the development of an open space or public park at the northwest corner of the intersection.

University Village Segment Plan  
Map 9



## EFFECTIVE ACTION PLAN

Successful implementation of this plan requires that specific measures, actions, programs, or techniques be taken in a timely, efficient, and cost-effective manner. The following implementation actions are specific tasks to be completed in order to facilitate the desired vision expressed within this plan.

Each action may be the responsibility of one or more entities and should be implemented in a given time frame. Time frames are short-term, medium-term, long-term, and ongoing. Ongoing efforts are those currently being undertaken and which should continue into the future. A short-term effort should begin upon plan adoption. Medium-term efforts should start 3 to 5 years after plan adoption, but could begin earlier if conditions allow for earlier implementation. Groundwork laid by short- and medium-term efforts must take place before many of the long-term efforts can be implemented 5 to 10 years after plan adoption.

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### MOBILITY ENHANCEMENT

- Investigate the possible installation of a roundabout at Butler Avenue and Shady Retreat Road.

**Entity Responsible:** Borough Council, Borough Planning Commission, Borough Administration

**Time Frame:** Short-term

- Improve the pedestrian circulation system through improvements to the sidewalk system and pedestrian crossings at street intersections.

**Entity Responsible:** Borough Council, Borough Planning Commission, Borough Administration

**Time Frame:** Ongoing

- Ensure that access management techniques are implemented as development occurs within the Borough.

**Entity Responsible:** Borough Council, Borough Planning Commission, Borough Administration

**Time Frame:** Ongoing

- Provide bike trail connections from the Borough to schools, parks, and community facilities and other trails.

**Entity Responsible:** Borough Council, Borough Planning Commission, Borough Administration

**Time Frame:** Ongoing

## DESIGN FOR MAIN STREET

- Ensure that streetscape techniques and gateway features are implemented as development occurs within the Borough.

**Entity Responsible:** Borough Council, Borough Planning Commission, Borough Administration

**Time Frame:** Ongoing

- Initiate the installation of the proposed gateway features.

**Entity Responsible:** Borough Council, Borough Planning Commission, Borough Administration

**Time Frame:** Long-term

- Pursue funding opportunities cited in Appendix 3 for streetscape improvements unrelated to proposed development.

**Entity Responsible:** Borough Council, Borough Planning Commission, Borough Administration

**Time Frame:** Ongoing

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## ECONOMIC DEVELOPMENT STRATEGIES

- Support the overall vision for the corridor. The Borough's efforts in economic development should support this vision. A common vision will also help lay the groundwork in establishing a marketable identity to be promoted.

**Entity Responsible:** Borough Council, Planning Commission, Community and Business Committee

**Time Frame:** Ongoing

- Support the Community and Business Committee as they undertake the following activities:

- Act as a champion for the corridor and the plan;
- Engage property and business owners to understand their needs and to further develop public-private partnerships;
- Promote the concepts and vision of the plan;
- Develop and coordinate marketing efforts; and
- Secure funding for improvements.

**Entity Responsible:** Borough Council, Planning Commission, Community and Business Committee

**Time Frame:** Ongoing

- Continue to seek out new businesses that are appropriate for the corridor as established within the market analysis.

**Entity Responsible:** Community and Business Committee

**Time Frame:** Ongoing

- Consider LERTA, BID or similar programs for portions of the corridor.

**Entity Responsible:** Borough Council, Community and Business Committee

**Time Frame:** Short-term

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## ORDINANCE CHANGES AND LAND USE POLICIES

- Require that all applications for change in use or expansion of uses go through the procedures to obtain land development and zoning approval so that the Borough can review the proposal and have a record of what is proposed.

**Entity Responsible:** Borough Council, Borough Planning Commission, Borough Administration

**Time Frame:** Ongoing

- Require that all necessary improvements such as buffering, landscaping, etc., be provided as a condition of the granting of approval for a change in use and expansion of use.

**Entity Responsible:** Borough Council, Borough Planning Commission, Borough Administration

**Time Frame:** Ongoing

- Implement historic preservation techniques for appropriate areas within the Historic Village segment (e.g., historic district, historic preservation overlay, etc.)

**Entity Responsible:** Borough Council, Borough Planning Commission, Borough Administration

**Time Frame:** Ongoing

- As part of the subdivision and land development ordinance, incorporate design guidelines for appropriate areas within the study area. (See Appendix 5 for Model Design Guidelines.)

**Entity Responsible:** Borough Council, Borough Planning Commission, Historic Preservation Committee, Borough Administration

**Time Frame:** Short-term

- Within the underlying zoning districts of the Mixed Use Overlay, limit types of commercial uses in the University Village Area by continuing the restricted commercial zoning designation. More intense commercial uses, such as service stations, car washes, pharmacies, convenience stores, and drive-in restaurants, should continue to be prohibited in this area. The existing uses can remain, but new uses that would have a detrimental effect on residences and the vision for the corridor should not be permitted.

**Entity Responsible:** Borough Council, Borough Planning Commission, Borough Administration

**Time Frame:** Ongoing

- Where changes or expansion in use are proposed, require that parking be located to the rear or side to improve the appearance of Butler Avenue.

**Entity Responsible:** Borough Council, Borough Planning Commission, Borough Administration

**Time Frame:** Ongoing

- Amend the zoning ordinance to permit apartments as part of individual commercial structures within the C-2 Commercial District.

**Entity Responsible:** Borough Council, Borough Planning Commission, Borough Administration

**Time Frame:** Short-term

- Amend the LI Light Industrial District to prohibit Adult Commercial, Automobile body repair or paint shop, Fuel storage and distribution, Light manufacturing, Telecommunications facility, Truck sales, Warehousing and distribution, and Wholesale trade on The Delaware Valley Concrete site (TMP #25-8-3-1).

**Entity Responsible:** Borough Council, Borough Planning Commission, Borough Administration

**Time Frame:** Short-term

- Determine if retail sales should be explicitly included with or excluded from the Automobile service station or car wash use which is currently permitted by Special Exception within the C-1 Commercial District.

**Entity Responsible:** Borough Council, Borough Planning Commission, Borough Administration

**Time Frame:** Short-term

- Amend the zoning ordinance to limit the size of retail trade and services in the C-1 and C-2 Commercial districts to 7,500 square feet to be consistent with the Mixed Use Overlay ordinance and to limit the size of retail buildings within the corridor.

**Entity Responsible:** Borough Council, Borough Planning Commission, Borough Administration

**Time Frame:** Short-term

- Consider reduction of the front yard setback requirements of the C-1 and C-2 Commercial Districts to more closely match the existing building locations.

**Entity Responsible:** Borough Council, Borough Planning Commission, Borough Administration

**Time Frame:** Short-term

- Amend the zoning ordinance to prohibit parking in the front yard along Butler Avenue and require parking to be placed behind the primary structure.

**Entity Responsible:** Borough Council, Borough Planning Commission, Borough Administration

**Time Frame:** Ongoing

- Amend the borough ordinances to require each commercial use to provide access easements for its parking aisles and driveways guaranteeing access to all abutting lots within the C-1, C-2, C-3 districts or within the Mixed Use Overlay District. These interconnections should be constructed during the initial land development.

**Entity Responsible:** Borough Council, Borough Planning Commission, Borough Administration

**Time Frame:** Ongoing

- Initiate review of the Student Housing Overlay Ordinance.

**Entity Responsible:** Borough Council, Borough Planning Commission, Borough Administration

**Time Frame:** Short-term

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## LAND USE PLAN

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### TOWN CENTER SEGMENT

- Initiate discussions with the owners of the Town Center of New Britain shopping center to facilitate improvements to the shopping center consistent with the recommendations found in the concept plans including improvements to the southwest corner of the shopping center, either the public space option or retail option, and interior site improvements.

**Entity Responsible:** Borough Planning Commission, Borough Administration

**Time Frame:** Medium-term

- Continue ongoing dialog with the owners of the Town Center of New Britain shopping center to be responsive to vacancy issues.

**Entity Responsible:** Borough Administration

**Time Frame:** Ongoing

- When development occurs within the Opportunity Area, ensure that this plan's recommendations are taken into consideration, including shared parking, building setbacks, sidewalks, required landscaping, and other elements that contribute to the streetscape.

**Entity Responsible:** Borough Council, Borough Planning Commission, Borough Administration

**Time Frame:** Ongoing

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#### *HISTORIC VILLAGE SEGMENT*

- Initiate project to install a sidewalk or macadam trail along Butler Avenue to connect Historic Village segment with the Town Center and University Village.

**Entity Responsible:** Borough Council, Borough Planning Commission, Borough Administration

**Time Frame:** Long-term

- Evaluate the existing trees and vegetation on the Access A and Access B islands to determine if replanting of street trees is needed.

**Entity Responsible:** Borough Council, Borough Planning Commission, Borough Administration

**Time Frame:** Medium-term

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#### *UNIVERSITY VILLAGE SEGMENT*

- Continue ongoing dialog with Delaware Valley University as revitalization of the University Village occurs to ensure that the symbiotic relationship between the Borough and University is strengthened.

**Entity Responsible:** Borough Council, Borough economic development consultant, Borough Administration

**Time Frame:** Ongoing

- Continue discussions with Doylestown Township on planning concepts for the University Village area, including the use of the SEPTA stations located in the township.

**Entity Responsible:** Borough Council, Borough economic development consultant, Borough Administration

**Time Frame:** Ongoing

- When development occurs within the Opportunity Area, ensure that the plan's recommendations are taken into consideration, including but not limited to, shared parking, building setbacks, sidewalks, required landscaping, and other elements that contribute to the streetscape.

**Entity Responsible:** Borough Council, Borough Planning Commission, Borough Administration

**Time Frame:** Ongoing

- Ensure that, if and when property owners choose to redevelop their properties (Delaware Valley Concrete, Delaware Valley University, Duck Deli, Bitzer's Cleaners, and Smith Marine/Alley-Gator Shop sites), development occurs with land uses that contribute to the vision for the University Village.

**Entity Responsible:** Borough Council, Borough Planning Commission, Borough Administration

**Time Frame:** Ongoing.